



**Notice of a public meeting of
Area Planning Sub-Committee**

To: Councillors Hollyer (Chair), Crawshaw (Vice-Chair),
Fisher, Galvin, Craghill, Melly, Orrell, Waudby, Webb and
Perrett

Date: Thursday, 20 August 2020

Time: 4.30 pm

Venue: Remote Meeting

AGENDA

Please note there will be no site visits ahead of this meeting.

1. Declarations of Interest

At this point in the meeting, Members are asked to declare:

- any personal interests not included on the Register of Interests
- any prejudicial interests or
- any disclosable pecuniary interests

which they may have in respect of business on this agenda.

2. Minutes

(Pages 1 - 10)

To approve and sign the minutes of the last meeting of the Area Planning Sub-Committee held on 5 August 2020.

3. **Public Participation**

At this point in the meeting members of the public who have registered to speak can do so. Members of the public may speak on agenda items or on matters within the remit of the committee.

Please note that our registration deadlines have changed to 2 working days before the meeting, in order to facilitate the management of public participation at remote meetings. The deadline for registering at this meeting is 5:00pm on Tuesday, 18 August 2020.

To register to speak please contact Democratic Services, on the details at the foot of the agenda. You will then be advised on the procedures for dialling into the remote meeting.

Webcasting of Remote Public Meetings

Please note that, subject to available resources, this remote public meeting will be webcast including any registered public speakers who have given their permission. The remote public meeting can be viewed live and on demand at www.york.gov.uk/webcasts.

During coronavirus, we've made some changes to how we're running council meetings. See our coronavirus updates (www.york.gov.uk/COVIDDemocracy) for more information on meetings and decisions.

4. Plans List

To determine the following planning applications:

Note: Annexed to each report is a series of presentation slides showing photographs of the site and its environs and plans of the proposed works.

- a) **Moorlands Nursing Home, 10 - 12 Moor Lane, Strensall, York, YO32 5UQ** (Pages 11 - 58)
[19/02044/FULM]

This application seeks permission for the erection of 62 bedroom care home with associated car parking and landscaping following the demolition of existing care home (resubmission of 18/02935/FULM) [Strensall]

- b) **61A Gale Lane, York, YO24 3AD** (Pages 59 - 90)
[20/00494/FULM]

This application seeks permission for the erection of 3 storey building to form 8no. apartments and 2no. semi-detached bungalows to the rear following demolition of existing bungalow with new access and associated landscaping. [Westfield]

- c) **Site to the rear of 5 Cherry Lane, York** (Pages 91 - 124)
[19/02729/OUT]

This outline application seeks permission for the erection of 5no. detached dwellings with means of access. [Dringhouses And Woodthorpe]

- d) **Etas Ltd. Bacchus House, Link Road, Court, Osbaldwick Link Road, Osbaldwick** (Pages 125 - 158)
[19/02738/FULM]

This application seeks permission for change of use from offices (Use Class B1) to health care unit (Use Class D1), and single storey extension and flat roof canopy to the north west elevation. [Osaldwick and Derwent]

5. Urgent Business

Any other business which the Chair considers urgent under the Local Government Act 1972.

Democracy Officer:

Name – Michelle Bennett

Telephone – 01904 551573

E-mail – michelle.bennett@york.gov.uk

For more information about any of the following please contact the Democratic Services Officer responsible for servicing this meeting:

- Registering to speak
- Business of the meeting
- Any special arrangements
- Copies of reports and
- For receiving reports in other formats

Contact details are set out above.

This information can be provided in your own language.

我們也用您們的語言提供這個信息 (Cantonese)

এই তথ্য আপনার নিজের ভাষায় দেয়া যেতে পারে। (Bengali)

Ta informacja może być dostarczona w twoim (Polish)
własnym języku.

Bu bilgiyi kendi dilinizde almanız mümkündür. (Turkish)

یہ معلومات آپ کی اپنی زبان (بولی) میں بھی میا کی جاسکتی ہیں۔ (Urdu)

 (01904) 551550

City of York Council

Committee Minutes

Meeting	Area Planning Sub-Committee
Date	5 August 2020
Present	Councillors Hollyer (Chair), Crawshaw (Vice-Chair), Cullwick, Fisher, Galvin, Melly, Orrell, Waudby, Webb, Perrett and Baker
Apologies	Councillor Craghill

There were no site visits due to COVID-19 restrictions.

66. Declarations of Interest

Members were invited to declare, at this point in the meeting, any personal interests not included on the Register of Interests, any prejudicial interests or any disclosable pecuniary interests that they might have in the business on the agenda.

Cllr Galvin declared a prejudicial interest in Agenda item 4a) 8 Harcourt Close, Bishopthorpe, [19/02653/FUL], in that he had called-in the application stating his concerns and had therefore predetermined his position. He left the meeting before consideration of that item and took no part in the debate or decision thereon.

67. Minutes

Resolved: That the minutes of the Area Planning Sub-Committee meeting held on 16 July 2020 be approved and then signed by the Chair at a later date.

68. Public Participation

It was reported that there had been no registrations to speak under the Council's Public Participation Scheme on general issues within the remit of the Sub-Committee.

69. Plans List

Members considered a schedule of reports of the Assistant Director, Planning and Public Protection, relating to the following

planning applications, outlining the proposals and relevant policy considerations and setting out the views of consultees and officers.

**70. 8 Harcourt Close, Bishopthorpe, York, YO23 2SW
[19/02653/FUL]**

[Cllr Galvin left the meeting having declared a prejudicial interest in this item].

Members considered a full application from Mr Frank Norbert for the erection of a detached bungalow in the side garden of the host dwelling with shared access from the existing drive.

Officers gave a presentation based upon the slides at pages 23 - 28 of the Agenda.

The Democracy Officer read out Mr Martin Dawson statement as he had been unable to do so due to technical issues. He had intended to speak in objection on behalf of his aunt who lived next door at No. 9. He raised concerns that building work would create disturbance in terms of noise levels, pollution and vehicle obstruction given that the area was a small cul de sac with limited parking, populated with a high proportion of elderly residents.

Mr Steven Thorpe, spoke in objection on the grounds that the proposal would create a crammed terraced effect, out of keeping with the neighbouring dwellings. The expanse of concrete would add additional strain upon the overburdened drainage system.

After debate, Cllr Crawshaw moved, and Cllr Webb seconded, that the application be approved in accordance with the officer recommendation. Members voted unanimously in favour of this motion and it was therefore:

Resolved: That the application be APPROVED, subject to the conditions listed in the report.

Reason: The proposal is for an infill house in a sustainable location. The design is compatible with its surroundings and it would not significantly harm the living conditions of adjacent dwellings. The proposals comply with

the National Planning Policy Framework and with Publication draft Local Plan (2018) policies D1, CC1, CC2 and ENV5, the draft Local Plan (2005) policies GP1 and GP10.

71. Dean Court Secure Car Park To Rear Of Portland Street York,[20/00505/FUL]

Members considered a full application from Mr B White for the erection of a two storey block for 9 apartments with associated cycle and refuse stores and part retention of existing car park.

Officers gave a presentation based upon the slides at pages 47 - 54 of the Agenda.

Ms June Tranmer, on behalf of the Guildhall Planning Panel, spoke in objection on the grounds that the proposed flats were too small for a suitable home for York residents, and would most likely end up as holiday flats. There would be no room for turning vehicles around in the remaining car park. There is a tree at the entrance to the car park that would probably be removed, with no plan to replace it. The boundary wall would further reduce the light into the gardens of the houses in Portland Street.

In response to questions from Members, officers confirmed that:

- The size of the units were the same as that which had been approved on their previous application in 2019.
- Although the units were smaller than the size of dwellings recommended in the national space standards, this standard had not been incorporated in to the Local Plan for York.
- Parking spaces would be rented out, therefore control over parking was not a conditioned.

After debate, Cllr Crawshaw moved, and Cllr Waudby seconded, that the application be approved, in accordance with the officer recommendation, with the amendment of conditions 8 and 16. Cllrs: Baker, Crawshaw, Cullwick, Fisher, Galvin, Melly, Orrell, Perrett, Waudby and Hollyer all voted in favour of this motion and Cllr Webb voted against it. It was therefore:

Resolved: That the application be APPROVED, subject to the conditions listed in the report with the following two amended conditions:

Amended Condition 8 Tree Protection

Prior to any groundworks on site details of tree protection measures for the Lime tree to the south-west of the application and the **tree to the south-east of the site** shall be submitted to and approved in writing by the Local Planning Authority. The development shall thereafter be carried out in accordance with the approved details.

The details shall include consideration of groundworks, including drainage, the installation of services, and the re-surfacing works. (It is noted that the previously submitted arboriculture report recommends that to ensure tree roots are not damaged during any resurfacing, the parking area will be surfaced over a cellular confined system which is to be specified within a separate Arboricultural Method Statement).

Reason:

In the interests of good design as required by paragraph 127 of the NPPF; to avoid damage to any trees which have amenity value and make a positive contribution to the character and appearance of the conservation area.

Amended Landscaping Condition 16

The development shall not be occupied until the species and stock size of the proposed trees (as shown on the approved plans) **and the management plan for the sedum roof** have been approved in writing by the Local Planning Authority.

The approved details and the landscaping scheme, as shown on the approved plans, shall be implemented within a period of six months of the completion of the development.

Any trees or plants which ~~within a period of five years from the completion of the development~~ die, are removed or become seriously damaged or diseased shall be

replaced in the next planting season with others of a similar size and species, **in perpetuity**, unless alternatives are agreed in writing by the Local Planning Authority.

Reason: To ensure the long term health of plants, trees and the sedum roof in support of the environmental objectives of the NPPF.

Reason for Approval: This scheme is similar to the 2018 application which Members recommended for approval. The difference is that 9 rather than 16 dwellings are proposed and the building would not be as long. It is concentrated towards the end of the site and some car parking would remain.

This is an underused urban site where in principle the NPPF recommends redevelopment, in particular development for which there is demonstrable need. The scheme would not have an adverse effect on the Central Historic Core Conservation Area and have no undue detrimental impact on neighbouring amenity. It does not raise any highway safety issues and other technical matters can be addressed through planning conditions.

72. Grimme (Uk) Ltd. Kilnfield House, 45 Common Road, Dunnington, York [20/00525/FULM]

Members considered a full application from Mr Alistair Kelly for permission for the change of use of premises comprising a combined industrial or storage and office building with yard and car parking to business, general industry and storage or distribution use (Use Classes B1, B2 and B8).

Officers gave a presentation based upon the slides at pages 67 - 72 of the Agenda and reported an additional representation from Cllr Warters, Ward Member for Osbaldwick and Derwent who considered the proposal to be unregulated B1, B2 and B8 usage, unrelated to Horticulture/Agriculture and inappropriate in

the Green Belt. He considered there to be no imperative to support this employment site given the extensive industrial estate opposite.

Mr Andrew Towleron, on behalf of Dunnington Parish Council, spoke in objection on the grounds that the proposal was an inappropriate development in the Green Belt conflicting with the emerging Dunnington Neighbourhood Plan that would have an adverse impact on the character and amenity of the area and neighbouring properties on Common Road and the buffer between the industrial area and residential properties as well as the safety and free flow of traffic on the surrounding roads.

Mr Mark Lane from DPP Planning, Agent for the applicant, explained that the applicant was relocating. This application had been made to avoid the building remaining vacant and to allow an appropriate range of uses to ensure the site continued to support the local economy. The proposal meets planning policy criteria with regard to buildings in the Green Belt and future uses and its priority for the re-use of buildings.

In response to questions from Members, officers confirmed that this was a speculative scheme, the committee are being asked to approve the use class. When the new user is in place they may apply to make changes, at which point conditions could be applied in terms of cycle storage, noise levels and such.

After debate, Cllr Webb moved, and Cllr Galvin seconded, that the application be approved, in accordance with the officer recommendation. Cllrs: Baker, Crawshaw, Galvin, Melly, Perrett, Hollyer and Webb all voted in favour of this motion and Cllrs: Cullwick, Orrell and Waudby all voted against this motion. Cllr Fisher abstained from voting. It was therefore:

Resolved: That the application be APPROVED, subject to the conditions listed in the report.

Reason: The site already has planning permission for office (B1) use and general industrial (B2) use. The B2 use is currently restricted to the importation and distribution of agricultural machinery. The application seeks to broaden the consent to all office/light industry (B1), general industry (B2) and storage/distribution (B8) uses. No external alterations are

proposed. The application complies with Green Belt policy and would support the local economy. Potential impact on local residents should be mitigated by conditions. The application complies with national planning policy in the NPPF and relevant policies of the emerging plan.

73. Corby (No.1) Unit Trust, Sovereign House, Unit 5, Kettlestring Lane, York, YO30 4XF [20/00146/FULM]

Members considered a full application from Mr George Cornwall-Legh for the erection of a building for light industry or general industry or storage/distribution or storage/distribution with ancillary trade counter use (use classes B1(c), B2, B8) and associated car parking and landscaping

Officers gave a presentation based upon the slides at pages 87 -92 of the Agenda and reported that an additional representation had been received from the Flood Risk Management Team (FRMT) who considered that the submitted geo-environmental appraisal was a phase 1 desk top survey only. That it did not include site specific ground investigation or infiltration testing, witnessed by FRMT. They advised that this should be carried out to discount the suitability of infiltration and for FRMT to agree in principle to the submitted drainage details. No evidence had been submitted to prove existing connected impermeable areas. FRMT put forward a number of conditions that would address these concerns if permission were granted. Of the suggested conditions Planning Officers recommended that the following two be added:

Additional Condition 19

The site shall be developed with separate systems of drainage for foul and surface water on and off site.

Additional Condition 20

Unless otherwise approved in writing by the local planning authority, there shall be no piped discharge of surface water from the development prior to the completion of the approved surface water drainage works and no buildings shall be occupied or brought into use prior to completion of the approved foul drainage works.

Ms Joanna Gabrilatsou, from JLL, planning consultant to the Industrial Property Investment Fund which is the applicant and a fund of Legal and General, explained that the application seeks to redevelop the site to provide a new unit which could be used for B1c, B2 or B8 use, creating 1,858 sq m of new floorspace to attract new/expanding business in York. The proposal is complicit with the objectives of the emerging Local Plan and the NPPF by delivering a sustainable employment use, potentially creating around 51 jobs.

In response to questions from Members, officers confirmed that:

- Meeting the BREEAM standard of 'Excellent' had been conditioned. If the applicant had difficulty in meeting that standard, it would be an opportunity for officers to advise them of how they could meet this target.
- The applicant had met the required number of car electrical charging points, it would therefore not be possible to condition more than that.

After debate, Cllr Webb moved, and Cllr Crawshaw seconded, that the application be approved, in accordance with the officer recommendation with an amendment to condition 5 and the addition of two conditions (referred to above). Members voted unanimously in favour of this motion and it was therefore:

Resolved: That the application be APPROVED, subject to the conditions listed in the report and the following amended / additional conditions:

Amended Condition 5

The development shall be carried out to a BRE Environmental Assessment Method (BREEAM) standard of 'Excellent'. A post-construction stage assessment shall be carried out and a post-construction stage certificate shall be submitted to the Local Planning Authority prior to occupation of the building (or in the case of the certificate as soon as practical after occupation). ~~Where it can reasonably be demonstrated that an excellent is not feasible, full justification for the lower rating shall be submitted to and agreed by the Local Planning Authority prior to occupation.~~ Should the development fail to achieve a BREEAM standard of 'excellent' or

the agreed alternative rating, a report shall be submitted for the written approval of the Local Planning Authority demonstrating what remedial measures should be undertaken to achieve the agreed standard. The approved remedial measures shall then be undertaken within a timescale to be approved in writing by the Local Planning Authority.

Reason: To ensure that the (BREEAM) standard of 'Excellent' is met prior to occupation.

Additional Condition 19

The site shall be developed with separate systems of drainage for foul and surface water on and off site.

Reason: In the interest of satisfactory and sustainable drainage.

Additional Condition 20

Unless otherwise approved in writing by the local planning authority, there shall be no piped discharge of surface water from the development prior to the completion of the approved surface water drainage works and no buildings shall be occupied or brought into use prior to completion of the approved foul drainage works.

Reason: So that the Local Planning Authority may be satisfied that no foul and surface water discharges take place until proper provision has been made for their disposal.

Reason for Approval: The redevelopment would support the local economy by providing employment floor space in keeping with the character of the area and in a sustainable location. The application complies with national planning policy in the NPPF and relevant policies of the emerging local plan.

Cllr Hollyer, Chair
[The meeting started at 4.30 pm and finished at 7.22 pm].

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COMMITTEE REPORT

Date: 20 August 2020 **Ward:** Strensall
Team: East Area **Parish:** Strensall With Towthorpe
Parish Council

Reference: 19/02044/FULM
Application at: Moorlands Nursing Home 10 - 12 Moor Lane Strensall York YO32 5UQ
For: Erection of 62 bedroom care home with associated car parking and landscaping following the demolition of existing care home (resubmission of 18/02935/FULM)
By: Mr M Ladhar
Application Type: Major Full Application
Target Date: 15 April 2020
Recommendation: Approve subject to Section 106 Agreement

1.0 PROPOSAL

1.1 The application site is the existing Moorlands Nursing Home, located on the western side of Moor Lane in Strensall. The existing nursing home, now closed, was built up from two bungalows at the site (no's 10 & 12) and has been extended a number of times to the rear. The surrounding area is residential, being characterised by a mix of one and two storey dwellings that are predominantly detached. Of note are two pairs of large Edwardian villas on the eastern side of Moor Lane, to the north east of the application site. There are a number of buildings set back from the highway frontage, including Harvest Close, directly to the north, Station Square to the west and Highlands Avenue to the south west. There are a number of mature trees nearby to the site including a protected Oak tree on Moor Lane.

1.2 Planning permission is sought for the demolition of the existing nursing home and its replacement with a 62 bed care home. An application was submitted in 2018 for the redevelopment of the site by the same applicant as care home but was withdrawn following officer concerns over design and an incomplete bat survey. The current application was submitted in October 2019 and following concerns raised by the case officer and local residents with regard to the scale and the impact on neighbour amenity, revised plans were submitted for consideration in February. These have been subject to a re-consultation with local residents and the relevant internal and external consultees.

1.3 The revised replacement care home is predominantly two storeys in height with the elevation fronting Moor Lane being approximately 8.6 metres in height with the front central gable being a further 0.75 metres higher. The width of the building is 39.9 metres to the front elevation. The total length rear from front elevation to rear is 57.5 metres. The existing vehicular access will be retained with a 25 space car park to the front of the building will be set behind a hedge. Cycle and waste storage is also provided. Space within the car park for an ambulance or doctor's vehicle is included. A landscaping scheme with grassed areas and a mix of planting is proposed.

2.0 POLICY CONTEXT

National Planning Policy Framework

2.1 The revised National Planning Policy Framework (NPPF) 2019 was published on 19 February 2019 and sets out the government's planning policies for England and how these are expected to be applied. The policies are material planning considerations.

2.2 The planning system should contribute to the achievement of sustainable development (Paragraph 7). To achieve sustainable development, the planning system has three overarching objectives; economic, social and environmental objectives. Paragraph 11 sets out that there is a presumption in favour of sustainable development.

Publication Draft Local Plan 2018

2.3 The Publication Draft City of York Local Plan 2018 ('2018 Draft Plan') was submitted for examination on 25 May 2018. Phase 1 of the hearings into the examination of the Local Plan took place in December 2019. In accordance with paragraph 48 of the NPPF the Draft Plan policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012. (NB: Under transitional

arrangements plans submitted for examination before 24 January 2019 will be assessed against the 2012 NPPF).

2.4 The evidence base underpinning the 2018 Draft Plan is capable of being a material consideration in the determination of planning applications.

2.5 Relevant Policies

H9	Older Persons Specialist Housing
HW7	Healthy Places
D1	Placemaking
D2	Landscape and Setting
D4	Conservation Areas
GI4	Trees and Hedgerows
ENV2	Managing Environmental Quality
ENV4	Flood Risk
ENV5	Sustainable Drainage
T1	Sustainable Access

2005 Development Control Local Plan

2.6 The Development Control Local Plan (DCLP) was approved for development management purposes in April 2005. Whilst the DCLP does not form part of the statutory development plan, its policies are considered to be capable of being material considerations in the determination of planning applications where policies relevant to the application are consistent with those in the NPPF albeit with very limited weight.

3.0 CONSULTATIONS

INTERNAL

Design Conservation and Sustainable Development (Conservation)

3.1 The development site is just outside the boundary of the Strensall Conservation Area and, therefore, affects its setting (how the heritage asset is experienced). Views from the south as you approach Strensall CA contribute to its character and appearance. The character is defined in the CA Appraisal as:

The Village has an attractive and informal mixture of well proportioned 18th Century and 19th Century detached houses and vernacular cottages. A sense of unity arises from their shared scale, height (2 storey) and use of traditional materials – mostly pinkish-brown brickwork with pantile or Welsh slate roofing. Buildings are positioned at slightly varying distances and angles to the street frontage, some gable end on to the street and most traditionally with small fronted gardens enclosed by walls, railings or hedges.

3.2 This defined character is clearly exhibited as you approach the CA from the south with the development site on your left. The current buildings although of no special interest at least exhibit a domestic scale that reflects this character.

3.3 Concerned that the current proposal doesn't sufficiently reflect the defined character of well-proportioned detached houses and vernacular cottages. The building is of a scale, in terms of height and footprint, which will have an institutional character. A scheme that is more articulated, in terms of its massing, and had the appearance of individual villas might be more appropriate. The proposal, in my opinion, would have a detrimental impact on the setting of the Conservation Area and would, therefore, reduce its significance.

Design Conservation and Sustainable Development (Landscape)

3.4 Note that the drainage run has been revised to keep it outside of the fenced protection area of the Oak T3 and Larch T4 at the front of the site; and a method for installing utilities is covered in the arboricultural method statement (AMS)

3.5 In respect of Oak T7, accept that the extent of tree surgery would be relatively small. Problems with leaves falling on the roof etc. would be no worse than the existing situation.

3.6 The working area is very tight, but given the existing footprint, relative to the proposed, accept that the methodology within the AMS sufficiently covers this element.

3.7 However, there will be new drainage runs within the root protection area (RPA) which could potentially cause additional root severance but it is difficult to judge without knowing what the existing drainage runs are. This is an area covered by the AMS, but would have to be assessed on site at the time of demolition/construction. I

note the AMS quite rightly includes the involvement of the arb' consultant where incursion into the RPA may be necessary.

3.8 Give the tight working area and the line of proposed drainage, it is strongly recommended that the development be pulled slightly further back to ease the risk of harm; however in this instance would not insist on this, bearing in mind the limited public visibility of the tree.

3.9 Nonetheless also very concerned about the quality of the living environment for the end unit – Standard Room 1 which is not only north-facing but would also be very dominated by the close presence of the Oak and Ash. The applicant should give this serious consideration. Furthermore, this creates a very real conflict and likely pressure to considerably cut back the crowns or remove the trees altogether. So again would strongly recommend that this unit is removed, or Standard room 2 is made bigger.

3.10 Thus, given the latest information, still suggest that the development be set further away from Oak T7, but leave that to planning officer weigh up the balance.

Design Conservation and Sustainable Development (Ecology)

3.11 A previous survey of this site identified building features suitable for roosting bats, and the site is located close to high quality foraging habitat. A bat emergence survey subsequently undertaken in June 2019 did not record any bats emerging from the building and no further survey work is recommended.

3.12 Although no bats were found to be roosting in the building its demolition will result in the loss of potential roosting habitat. The increased structural integrity of modern developments reduces the potential for bats to utilise modern buildings for and roosting therefore any new developments should integrate a variety of features or bat boxes. Condition recommended.

Highways Network Management

3.13 Reviewed the revised design for this application, noting that there is an existing planning consent for the same use.

3.14 As per previous comments, please can we condition a Travel Plan and CEMP/Method of work and a financial contribution/bond to manage TROs/parking

on Moor Lane, including to protect the grass verges, if overflow parking from the site becomes an issue (approx.. £5,000).

3.15 Note that deliveries are an issue raised in multiple objections. The design of the car park should enable deliveries to take place on site but this could also be managed through a servicing plan to be conditioned.

3.16 The cycle parking design is improved for staff and now acceptable. The visitor hoops should be moved to a more suitable location as the allocated space near parking bay 25 is very narrow. Would this work better near bays 14 or 15?

3.17 I note that the bin store has moved to what appears to be a very tight location. I am not sure there is enough space to get large bins through at the side of the building and in the car park for them to be collected.

Environmental Protection Unit

3.18 Conditions recommended for construction hours, Construction Environmental Management Plan (CEMP), air quality, machinery and plant noise, deliveries and waste collection, odour, light and contaminated land.

Flood Risk Management Engineer

3.19 No response received.

Older Persons Accommodation Programme

3.20 The scheme is for a 62 bed care home in Strensall. Strongly in support of this increase in provision in the north of the city.

3.21 National Benchmarks for the provision of care home beds indicate that for every 100 residents aged 75+ there should be 11 residential and/or nursing care beds available.

3.22 York currently has 1300 care home beds, (with a further 158 in development) a statistical shortage of 576 beds. The city's care homes are currently operating at approximately 98% capacity, which puts significant pressure on the sector.

3.23 In the north of the city there are 432 care home beds which is 9.08 care home beds per 100 residents aged 75+ in that area of the city. Increasing this provision by 62 beds will increase the overall rate of provision in the north to 10.39 beds per 100 residents aged 75+. This development will help to address the shortage of provision in the city.

3.24 The scheme offers a mixture of bedroom types to cater for residents on a range of budgets, and shared communal, social areas. There is access to the outdoor garden area from the communal lounge. It would be good to have further information about how the outdoor space will be laid out and how resident's access to this space will be supported and encouraged.

EXTERNAL

Strensall Parish Council

3.25 Object for the reasons following:

- Scale and massing - effect on amenity of neighbouring properties
- Use of terraces/balconies - effect on amenity of neighbouring properties
- Car parking - which is inadequate for number of staff/visitors
- It is considered that the proposal, by virtue of the size and extent of the building footprint and its excessive scale and massing, would adversely affect the amenity and outlook of the occupiers of adjacent residential properties and would be unduly harmful to the character and appearance of the area. Thus it would conflict with Central Government advice on design contained within Planning Policy Statement 1 ("Delivering Sustainable Development") and policies GP1, GP10 and C1 of the City of York Draft Local Plan
- The lack of a Daylight & Sunlight Assessment demonstrates that the developer has not assessed the impact of natural light and potential environmental effects this application would cause. It is noted in the City of York Council: House Extensions and Alterations Draft Supplementary Planning Document (December 2012) that overshadowing and loss of light is covered in sections 4.1, 4.2, 4.3, 4.4, 4.5, 4.6; and

- (If members are minded to approve) The premises shall be used only as a residential care home for older people within Use Class C2 and shall not be used for any other purpose, including any other purpose in Class C2 of the Schedule of the Town and Country Planning (Use Classes) Order 1987, as amended, or in any provision equivalent to that Class in any Statutory Instrument revoking and re-enacting that order.

3.26 For the avoidance of doubt, older people are defined as over 55 years of age. This is in order to allow consideration of the impact of any changes on amenity, and because the consideration of the planning application has taken account of the need for older persons accommodation.

- Application 09/01776/OUT which was refused and dismissed at appeal (APP/C2741/A/10/2126146) that has clear parallels to this application.

3.27 Response to revised plans – February 2020 - The Parish Council feel the minor alterations do not affect the substance of their previous comments and due consideration should be taken of the neighbours current and previous comments.

Yorkshire Water

3.28 The Surface Water Drainage Strategy is acceptable. Condition recommending separate foul and surface water drainage recommended.

Foss Internal Drainage Board

3.29 Condition recommended to ensure surface water discharged to public sewer adopted by the water company or alternative method demonstrated if this is not viable.

North Yorkshire Police Designing Out Crime Officer

3.30 The overall design and layout of the proposed scheme is considered acceptable.

4.0 REPRESENTATIONS

4.1 The application has been advertised by neighbour notification, press notice and site notice. 17 letters of objection were received.

4.2 The issues raised in the objections are summarised below.

- Noise during construction
- Loss of sunlight, make house and garden feel dark, overshadowing
- Loss of privacy
- Increase in traffic
- Insufficient parking
- Loss of value if going to sell house
- Scale is excessive and overbearing
- Terraces will result in loss of privacy, noise particularly in summer
- Insufficient parking, Moor Lane will become overspill car park
- Noise and smell from kitchen
- Noise, disturbance, and pollution from increased vehicular traffic from staff, service/delivery vehicles and visitors.
- Not on a bus route, nearest bus route 10 minute walk away
- Light pollution
- Flooding/drainage concerns, existing drainage concerns on Moor Lane
- Industrial scale, unsuitable for location
- Too high for area
- Don't object to a care home on the site, but a more suitable design required
- Previous care home had difficulties recruiting staff
- Concerns over scale and mass
- Wildlife
- Height would tower over Harvest Close
- Noise pollution if there is a dementia unit
- Out of scale looks more like a hotel
- no daylight/sunlight assessment been undertaken
- does doctor surgery have capacity
- first floor windows will lead to overlooking and noise than existing layout

4.3 Revised plans were received and were subject to re-consultation in February 2020. Twelve letters of objection were received. The following concerns were raised.

- changes seem minimal and previous objections remain
- note 2 additional parking spaces but can't see bike and waste store
- still concerns over scale and design,
- overlooking and loss of light, drainage concerns on Moor Lane,
- noise and parking
- 3 storeys too high
- Staffing levels an issue

- Why not build it on Fours Alls site
- Where will lorries offload

5.0 APPRAISAL

5.1 Main Issues

- legal context
- need for older person's accommodation
- impact on the character of the conservation area
- impact on the streetscene
- impact on amenity and living conditions of adjacent occupiers
- highways and parking
- ecological issues
- drainage

LEGAL CONTEXT

5.2 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that planning applications should be determined in accordance with the development plan unless material considerations indicate otherwise. There is no development plan for this part of the City of York.

5.3 The application site lies adjacent to the southern end of the Strensall Village Conservation Area where Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires that special attention be paid to the desirability of preserving or enhancing the character or appearance of that conservation area.

NEED FOR OLDER PERSONS ACCOMMODATION

5.4 The current planning use of the application site is that of a care home, albeit now closed. As there is no change of use of the site proposed, the principle of development is acceptable.

5.5 Paragraph 59 of the NPPF states that to support the Government's objective of significantly boosting the supply of homes, it is important that a sufficient amount and variety of land can come forward where it is needed and that the needs of groups with specific housing requirements are addressed.

5.6 Policy H9 of the 2018 Draft Plan states that developments specifically designed to meet the accommodation needs of older people will be supported where they:

- i. contribute to meeting an identified need;
- ii. are well designed to meet the particular requirements of residents with social, physical, mental and/or health care needs; and
- iii. are in an accessible location by public transport or within walking distance to a range of community facilities including shops, medical services and public open spaces or these are provided on-site.

5.7 The Council's Older Person's Accommodation Programme has highlighted the existing shortfall of provision for care homes in the north of the city. The proposed development is considered to meet parts i. and ii. of Policy H9 as there is an identified need for older persons accommodation and the design incorporates facilities for residents such as outdoor amenity space, roof terraces and further facilities such as an ambulance parking space.

5.8 In terms of part iii. of policy H9 (accessibility) the application site is approximately 550 metres from the bus stop and facilities on The Village, including shops and public houses and 600 metres from Strensall Medical Practice on Southfields.

5.9 The proposal is therefore considered to be in accordance with paragraph 59 of the NPPF and Policy H9 of the 2018 Draft Plan. Given the identified shortfall of care home places, the redevelopment of the care home to provide a modern replacement is an identifiable public benefit.

IMPACT ON THE CHARACTER OF THE STRENSALL VILLAGE CONSERVATION AREA

5.10 Paragraph 192 of the NPPF states that local planning authorities should take account of the following when determining planning applications:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- c) the desirability of new development making a positive contribution to local character and distinctiveness.

5.11 Paragraph 193 states when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

5.12 Paragraph 196 states where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

5.13 Policy D4 of the 2018 Draft Plan advises that affecting the setting of a conservation area will be supported where they:

- i. are designed to preserve or enhance the special character and appearance of the conservation area and would enhance or better reveal its significance;
- ii. respect important views; and
- iii. are accompanied by an appropriate evidence based assessment of the conservation area's special qualities, proportionate to the size and impact of the development and sufficient to ensure that impacts of the proposals are clearly understood.

5.14 The draft policy goes on to say that harm to buildings, open spaces, trees, views or other elements which make a positive contribution to a Conservation Area will be permitted only where this is outweighed by the public benefits of the proposal.

5.15 The application site lies to the south west of Strensall Village conservation area. The conservation area was first designated in 1979 and later extended south from Princess Road to include the eastern side of Moor Lane, stopping approximately at the north eastern corner of the application site. The character appraisal was published in 2010 and notes in paragraph 10.02 that "the special interest of this character area is the result of its development during the late nineteenth and early twentieth century. As a result, this area has a distinct suburban character, very different from the traditional village qualities of the rest of the Conservation Area." Nos. 5-11 Moor Lane are highlighted as excellent examples of large, early twentieth century, semi-detached suburban houses. The grass verge down the west side of Moor Lane also highlighted as important to the streetscene as

it softens the hard landscape and includes several mature trees, which contribute to the leafy appearance of this character area.

5.16 The application site lies outside of the conservation area, thus reducing the significance of the designated asset and will result in an increased set back from the public highway compared with the existing building. While the proposed development is larger than existing in terms of the height and the width, there is a much increased set back and the grass verge is maintained. It is noted in the conservation appraisal that the character of the closest part of the conservation area is 'suburban' rather than 'traditional village'. While the Conservation Architect has identified there will be harm to the character of the conservation area due to the scale of the development given the domestic nature of the existing buildings. The level of harm identified is considered to be modest in its significance due to the location of the proposal outside of the conservation area, the increased set back of proposed buildings away from the conservation area and the retention of the grass verge on Moor Lane. The identified harm is considered to be less than substantial and it is necessary to consider the public benefits of the proposal as per paragraph 196 of the NPPF.

IMPACT ON THE STREETSCENE

5.17 Paragraph 127 of the NPPF states that planning decisions should ensure that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and

where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

5.18 Policy D1 of the 2018 Draft Plan states that proposals will be supported where they improve poor existing urban and natural environments, enhance York's special qualities and better reveal the significances of the historic environment.

Development proposals that cause damage to the character and quality of an area will be refused.

5.19 The existing care home has evolved in an ad hoc manner over recent decades with the result being somewhat awkward in appearance when viewed from the front and an inefficient use of the space. The existing layout is poor with no suitable amenity space for residents and the applicant advises that existing bedrooms do not have en suite facilities. The existing building is considered to be of no architectural merit. The layout is clearly inefficient from an operational perspective for staff. The proposed new care home will result in a larger building than previously, in terms of the height although a similar number of bedrooms will be provided. Multiple objections have been received that the scale is excessive for the character of the area.

5.20 The proposed replacement care home is for a single T shaped building which is predominantly two storeys in height with a three storey element in the roof of the front elevation. The dual pitched roof of the building will reflect the character of the area and the gables to the front elevation reflect those used in the villas on the opposite side of Moor Lane. The use of buff brick and grey/red riviis roof tiles would be in keeping with the mixed palette of materials found elsewhere along Moor Lane. It is proposed that samples of materials are to be required by condition to ensure a satisfactory appearance.

5.21 The revised design height of the building has been reduced from a front elevation roof ridge height of 9.6 metres for the withdrawn 2018 application to 8.6 metres with the revised plans submitted in 2020, excluding a slight increase for the central front gable. This is a reasonable height for a two storey pitched roof building, such as a dwelling. For example a recent planning application at 2 Harvest Close (next but one to the north) of the site show a height of 8.7 metres for the dwelling (18/01733/FUL). Given there are a number of two storey buildings on Moor Lane, the height is considered acceptable.

5.22 More concerns were raised over the width and depth of the building to the front elevation and the rear projection. The front elevation is approximately 40 metres in width, however there is a substantially increased set back from the front elevation when compared to the existing building, meaning this will mainly be apparent when viewing the building from directly in front and from the north east. It is correct that the scale is larger than adjoining buildings, however given the use this is to be expected. It will sit within a relatively mixed streetscene including a mix of one and two storey dwellings and also three storey villas on the eastern side of the road. There is also a row of terraced housing the west of the site on Station Square. The reduction of the height of the building to 8.6 metres is comparable to, or lower than, many surrounding buildings and this helps to mitigate the length of the development.

5.23 The character of the area is also defined by the generous grassed verges on highway edge to the west of which this proposal will have a positive impact by being further removed from the highway. The existing layout has a much larger footprint than the proposal but is for the most part single storey. It does however sit very close to the boundaries of the site, approximately 3 metres. While the proposal is predominantly two storey the rear off shoot has been brought in from the boundaries and is a minimum of 7 metres from the northern boundary and normally at least 8.5 metres from the northern boundary. It is considered that the substantial set in from most of the boundaries would help to mitigate the length of the rear off shoot.

5.24 The existing landscaping of the building is predominantly hardstanding in a courtyard arrangement and detracts from the appearance of the site. The proposed replacement scheme increases the level of the soft landscaping and is a positive of the scheme. The applicant has also set out how the roof terraces and the improved landscaping scheme are important for the physical and mental wellbeing of residents. The Council's Landscape Architect has noted the proximity of the tree on the western boundary to the site and that this may increase pressure for tree works in the future and have an impact on the level of light to the closest room. While they have recommended the building be pulled further away, they have not objected on these grounds, noting the lack of visibility of the tree and the impact of the existing building. The Landscape Architect also notes the impact of the existing hardstanding on trees on the site resulting raised tarmac in places. The proposed landscaping scheme is considered to be beneficial in this respect.

5.25 Objectors have referenced the appeal for a care home that was dismissed in 2010 at the Bonneycroft site further to the north on Moor Lane. This appeal was

against the refusal of outline planning permission for a care home on the site of an existing bungalow and was for a 50 – 60 bed care home in an 'I' shape layout of 2, 2.5 and 3 storeys in height. The appeal was dismissed on the grounds of the impact on the character of the area and on neighbour amenity, with the Inspector noting concerns over the impact on the character of the conservation area. The site has subsequently been redeveloped as a residential cul de sac of 7 dwellings (13/00566/FUL). It is noted that the application site is an existing care home site with a similar number of bedrooms, rather than a site containing a single bungalow. As such the context differs, the application site is already substantially more developed than Bonneycroft was and there is no change of use proposed.

IMPACT ON NEIGHBOUR AMENITY

5.26 The NPPF states that developments should create places with a high standard of amenity for all existing and future users. It goes on to state that decisions should avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development. Policies D1 and ENV2 of the 2018 Draft Plan consider amenity.

5.27 The use of the site as a care home will remain unchanged. Concerns have been raised by adjoining neighbours over a loss of light and privacy as a result of the development. There have also been concerns regarding noise and comings and goings.

5.28 In terms of impact of loss of light, the proposal is a detached building that is predominantly two stories in height. Furthermore it has been reduced in height during the application process in response to comments from neighbours and concerns from officers. The rear off shoot of the building is set well in from the side boundaries of the site, unlike the current building. The most affected buildings are likely to be the dwellings directly to the north, particularly 1 Harvest Close. There are no habitable windows to the side facing elevation of 1 Harvest Close and the proposed building would be a minimum of 8.5 metres from the rear garden of this dwelling. Due to the kinked design of the off shoot, it would be much further away from the rear of the garden. The submitted sunlight assessment indicates that there would not be an unacceptable loss of light to the garden. Furthermore the closest first floor bedroom window will be obscure glazed to assist in protecting privacy. It is not considered the proposal would result in an unacceptable loss of privacy or light to no 1 or would be overbearing.

5.29 No 8 Moor Lane lies to the north east of the proposed building and would due to the designed set back retain an acceptable level of light to the garden. Concerns have also been raised by residents to the west and south of the building however the building is two storey and the separation distances are considered to be acceptable. The closest buildings lie directly to the south on Moor Lane. However due to the design of the rear off shoot being over 20 metres from these houses and the location of the building to the north there would be no loss of sunlight or privacy.

5.30 There are two roof terraces and these are set well in from the boundaries and would have screens to reduce noise pollution. Noise pollution has been raised as a concern with regard to comings and goings and the additional storey to the building. In terms of comings and goings the proposal would be in the same use class as previously and would have a similar number of occupiers. A condition is proposed to restrict delivery times.

5.31 In terms of light pollution, it is noted the site lies within the existing settlement boundary and as such a building in residential use will produce some light, like its neighbours. The Council's Public Protection Officer has no objection subject to a condition controlling lighting through a curfew from 11pm to 7am and also the submission of a lighting assessment for proposed lighting. A Construction and Environmental Management Plan (CEMP) condition is recommended to minimise disruption during the construction process. Conditions are also proposed to cover remediation for gas safety, for the finding of unexpected land contamination and to cover details of external noise and odour from the kitchen.

HIGHWAY SAFETY AND PARKING

5.32 Paragraph 108 of the NPPF states that when assessing applications for development, it should be ensured that:

- appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location
- safe and suitable access to the site can be achieved for all users; and
- any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree.

5.33 Para 109 goes onto say development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

5.34 The site is located on a relatively straight section of Moor Lane, with an existing access that has good visibility due to the extensive grass verges on the western side of the public highway. It is noted there is a railway level crossing to the north of the site but it is approximately 100 metres distant. The proposed care home will utilise the existing access for the care home and further more will not result in an increase in residents at the site. Secure cycle parking and dedicated disabled and ambulance parking will be provided. Following comments from the Highways Officer regarding visitor cycle parking and access for large bins, a revised site plan has moved the visitor spaces. The access has been confirmed as being 0.3 metres wider than the bins used. The Council's Highways Officer has no objection to the proposal subject to conditions. Given the use of the site will remain as before and the number of bedrooms will be decreased the proposal is considered to be acceptable with regards to highway safety and parking. It is proposed that a Section 106 is entered into with the applicant to monitor parking levels on the verges of Moor Lane and to set out remedial measures if required in the form of traffic regulation order (TRO). The applicant has agreed in principle to an up to £5,000 contribution to cover a TRO repayable after 5 years if overspill parking is not an issue.

ECOLOGY

5.34 Paragraph 175 of the NPPF states that when determining applications, significant harm to biodiversity resulting from a development should be avoided or mitigated or an application should be refused. Policy GI2 of the Draft Plan seeks to conserve and enhance York's biodiversity. Where appropriate, any development should result in net gain to, and help to improve, biodiversity.

5.35 A Bat Emergence Survey and Bat Risk Assessment Report were submitted alongside the resubmitted application. The survey from June 2019 did not record any bats emerging from the building and no further survey work is recommended. A condition is recommended for bat roosting features to be incorporated into the new building to provide an ecological improvement.

DRAINAGE

5.36 The application site lies in flood zone 1 and currently contains a substantial amount of impervious hardstanding. While it is recognised that there are issues with surface water drainage in the local area due to soil conditions, the proposal introduces additional soft landscaping and grassed areas onto the site and the proposal can be controlled by a pre-commencement condition. It is noted there are no objections from Yorkshire Water or the Internal Drainage Board.

SUSTAINABILITY

5.37 The applicant has been advised of the requirements of policies CC1 and CC2 of the 2018 Draft Plan with regard to climate change. A carbon and renewable energy report has been submitted which indicates the policies can be met. A condition will be imposed to ensure policy compliance. Given the ad hoc and single storey nature of the existing care home, the redevelopment will result in a more energy efficient and modern building and facilities for the residents. As such the climate change policy requirements for redeveloping the site are an identifiable public benefit.

6.0 CONCLUSION

6.1 Paragraph 11 of the NPPF states that planning decisions should apply a presumption in favour of sustainable development. This means approving development proposals where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, unless the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework taken as a whole.

6.2 The proposed development does not involve a change of use given the use would remain as a care home. A need for places in care homes in this area of York has been recognised and the replacement would allow these places to be kept. The redevelopment of the site would provide a much more efficient and suitable modern building and greatly improved outdoor amenity space for residents. While concerns have been raised over the design and scale of the building, the height is similar to existing buildings in the immediate area. The size of the building is acknowledged but due to the reasonable height, the substantial set back from the public highway and the set in front the adjoining properties it is considered to be acceptable and

broadly in accordance with national and local policies with regard to design. In terms of amenity, the use and number of occupiers will remain as before and due to the reduced height and siting of the majority of the building away from the boundary, impacts will be minimised. Obscure glazing will be applied to certain windows to protect privacy. The proposal is considered to be acceptable in terms of highway safety and parking subject to condition. The proposal will also result in a more modern building that achieves the climate change policies set out in CC1 and CC2 of the 2018 Draft Plan. The site layout shows there will be an increase in soft landscaping which in turn is likely to have a beneficial impact in terms of drainage.

6.3 Set against this is the identified harm to the character of the Strensall Conservation Area by the siting of the building to the south west of the conservation area boundary. The building will be of a larger scale than previously and will be more visible in views out of the conservation area. However the amount of harm is considered to be modest rather than significant. The NPPF states that great weight should be attached to an identified harm to a designated heritage asset and the greater the importance of the asset, the greater the weight. Given the proposal affects the character of the conservation area from views to the outside, this is considered to further reduce the weight against granting permission to be apportioned in the planning balance. Furthermore, the harm should be weighed against the public benefits of a modernised, replacement care home with improved facilities and an improved landscaping scheme for residents in an area of recognised need for care home place. The proposed design will be more energy efficient in terms of meeting the climate change policies in the 2018 Draft Plan and will result in an increased permeable area for drainage with a new drainage strategy. There are further limited public benefit in the form of temporary employment during the construction period and measures for bat habitation incorporated into the design. The identified harm is not considered to outweigh these identified benefits. The proposal is therefore considered to be acceptable and is recommended for approval subject to the following conditions and a legal agreement to cover the monitoring of parking on the public highway.

7.0 RECOMMENDATION: Approve subject to Section 106 Agreement

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans:-

3033.07.131 G received 26 June 2020
3033.07.140 J received 26 June 2020
3033-07-101L received 21 May 2020
4566M-101 REV C received 17 March 2020
AMS-TPP A received 17 March 2020
3033.07.130 G received 21 Feb 2020
3033.07.132 G received 21 Feb 2020
3033.07.141 H received 21 Feb 2020
3033.07.100B received 23 September 2019

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 DRAIN1 Drainage details to be agreed

4 VISQ8 Samples of exterior materials to be app

5 Before the commencement of development, including demolition, and throughout development operations, tree protection shall be implemented in strict accordance with Arboricultural Method Statement (insert revision number) and AMS Tree Protection Plan (TPP) (insert revision number) by All About Trees. A copy of the documents will be available for reference and inspection on site at all times.

Reason: To protect existing trees which are covered by a Tree Preservation Order and/or are considered to make a significant contribution to the amenity of this area and/or development.

6 Before the commencement of development, construction details and an accompanying method statement for the reinstatement of the car park surface and edging within the root protection areas of the existing trees shown to be retained shall be submitted to and approved in writing by the Local Planning Authority. The approved details shall be adhered too throughout the construction period.

Reason: To protect existing trees which are covered by a Tree Preservation Order and/or are considered to make a significant contribution to the amenity of this area and/or development.

7 Within three months of commencement of development a detailed landscape scheme shall be submitted to and approved in writing by the Local Planning Authority. This shall include the species, stock size, density (spacing), and position of trees, shrubs and other plants; and seed mixes, sowing rates and mowing regimes where applicable. The proposed tree planting shall be compatible with existing and proposed utilities. This scheme shall be implemented within a period of six months of the practical completion of the development. Any trees or plants which within a period of

five years from the substantial completion of the planting and development, die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of a similar size and species, unless the Local Planning Authority agrees alternatives in writing.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species and other landscape details across the site, since the landscape scheme, is integral to the amenity of the development and the immediate area.

8 Prior to the development coming into use details of the cycle parking areas, including means of enclosure, shall be submitted to and approved in writing by the Local Planning Authority. The building shall not be occupied until the cycle parking areas and means of enclosure have been provided within the site in accordance with such approved details, and these areas shall not be used for any purpose other than the parking of cycles.

Reason: To promote use of cycles thereby reducing congestion on the adjacent roads and in the interests of the amenity of neighbours.

9 No part of the development shall be occupied until a Full Travel Plan has been submitted to and approved in writing by the Local Planning Authority. The site shall thereafter be occupied in accordance with the aims, measures and outcomes of said approved Travel Plan.

Reason: To ensure that traffic flows from the site can be safely accommodated and to promote the usage of sustainable means of transport.

10 Prior to commencement of the development, a Construction Environmental Management Plan (CEMP) for minimising the creation of noise, vibration and dust during the demolition, site preparation and construction phases of the development shall be submitted to and approved in writing by the Local Planning Authority. The CEMP must include a site specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and include a package of mitigation measures commensurate with the risk identified in the assessment. All works on site shall be undertaken in accordance with the approved scheme, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To protect the amenity of the locality

11 HWAY40 Dilapidation survey

12 Except in case of emergency no demolition and construction works or ancillary operations, including deliveries to and dispatch from the site which are audible beyond the boundary of the site shall take place on site other than between the hours of 08:00-

18:00 Monday to Friday and between 09:00-13:00 on Saturdays.

The Local Planning Authority shall be notified at the earliest opportunity of the occurrence of any such emergency and a schedule of essential work shall be provided.

Reason. To protect the amenity of local residents

13 The development shall not be occupied until a minimum of 1 parking bay has incorporated facilities for charging electric vehicles. Electric Vehicle Recharging Points (minimum 32A) shall be provided in a position and to a specification to be first agreed in writing by the Council. In addition, a minimum of 1 additional parking bay should be identified for the future installation of additional Electric Vehicle Charging Points. Such additional bays should be provided with all necessary cabling and groundwork to facilitate the addition of Electric Vehicle Charge Points in the future. The locations of these additional bays should also be agreed in writing by the Council. Within 3 months of the first occupation of the development, an Electric Vehicle Recharging Point Management Plan shall be submitted to the Council for approval in writing. The Electric Vehicle Recharging Point Management Plan will detail the management, maintenance, servicing and access arrangements for each Electric Vehicle Recharging Point for a period of 10 years.

Reason: To promote and facilitate the uptake of electric vehicles on the site in line with the Council's Low Emission Strategy (LES) and the National Planning Policy Framework (NPPF).

14 Prior to the commencement of above ground construction, details of all machinery, plant and equipment to be installed in or located on the premises, which is audible outside of the premises, shall be submitted to the local planning authority and approved in writing. These details shall include average sound levels (LAeq), octave band noise levels and any proposed noise mitigation measures. The machinery, plant or equipment and any approved noise mitigation measures shall be fully implemented and operational before the development comes into use and shall be appropriately maintained thereafter.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

Note: The combined rating level of any building service noise associated with plant or equipment at the site should not exceed the representative LA90 1 hour during the hours of 07:00 to 23:00 or representative LA90 15 minutes during the hours of 23:00 to 07:00 at 1 metre from the nearest noise sensitive facades when assessed in accordance with BS4142: 2014, inclusive of any acoustic feature corrections associated with tonal, impulsive, distinctive or intermittent characteristics.

15 Upon completion of the development, delivery vehicles and waste removal vehicles to the development shall be confined to the following hours:

Monday to Friday 08:00 to 18:00 hours

Saturday 09:00 to 13:00 hours and not at all on Sundays and Bank Holidays

Reason: To protect the amenity of occupants of the nearby properties from noise.

16 There shall be adequate facilities for the treatment and extraction of cooking odours. Details of the extraction plant or machinery and any filtration system required shall be submitted to the local planning authority for written approval. Once approved it shall be installed and fully operational in accordance with the approved details before the development comes into use and shall be maintained and serviced thereafter in accordance with manufacturer guidelines.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

Note: It is recommended that the applicant refers to the updated Guidance produced by EMAQ in September 2018 titled "Control of Odour and Noise from Commercial Kitchen Exhaust Systems (September 2018)" for further advice on how to comply with this condition. The applicant shall provide information on the location and level of the proposed extraction discharge, the proximity of receptors, size of kitchen or number of covers, and the types of food proposed. A risk assessment in accordance with APPENDIX 3 of the EMAQ guidance shall then be undertaken to determine level of odour control required. Details should then be provided on the location and size/capacity of any proposed methods of odour control, such as filters, electrostatic precipitation, carbon filters, ultraviolet light/ozone treatment, or odour neutraliser, and include details on the predicted air flow rates in m³/s throughout the extraction system.

17 There shall be no external lighting between the hours of 23:00hrs and 07:00hrs by obtrusive lights, other than those necessary for emergency and security lighting. A scheme outlining which lights will be switched off between these hours shall be submitted to and agreed by the Local Authority in writing before use of the site commences and the curfew implemented in accordance with the said scheme as approved thereafter.

Reason: To protect the amenity of the locality.

18 The development shall not be occupied until a full Lighting Impact Assessment undertaken by an independent assessor detailing predicted light levels at neighbouring residential properties including a description of the proposed lighting, a plan showing vertical illuminance levels (Ev) and all buildings within 100 metres of the

edge of the site boundary

Artificial lighting to the development must conform to requirements to meet the Obtrusive Light Limitations for Exterior Lighting Installations for Environmental Zone E3 contained within the following table taken from the Institute of Light Professionals Guidance Notes for the Reduction of Obtrusive Lighting.

Reason: To protect the amenity of the locality.

19 Prior to commencement of development, gas monitoring and/or a risk assessment shall be carried out by a competent person to assess landfill gas generation and migration. Based on the results of the gas monitoring and/or risk assessment, the detailed design of a gas protection system shall be submitted to and approved by the local planning authority.

Reason: To ensure that risks from landfill gas to the future users of the land and neighbouring land are minimised and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

20 The development shall not be occupied until, a verification report that demonstrates the effectiveness of the gas protection system shall be submitted to and approved in writing by the local planning authority.

Reason: To ensure that risks from landfill gas to the future users of the land and neighbouring land are minimised and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

21 LC4 Land contamination - unexpected contam

22 Prior to first occupation or use of the development hereby approved four integrated features providing a roosting crevice for bats must be constructed within the fabric of the new buildings.

Reason: To take account of and enhance the biodiversity and wildlife interest of the area, and to be in accordance with Paragraph 175 d) of the NPPF (2019) to encourage the incorporation of biodiversity improvements in and around developments, especially where this can secure measurable net gains for biodiversity.

23 Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2018 (or any order revoking or re-enacting that Order), unless otherwise agreed in writing with the Local Planning Authority, the windows identified as obscure glazed in approved drawing 3033.07. 131rev G shall at all times be obscure glazed to a standard equivalent to Pilkington Glass level 3 or above and

remain fixed shut below 1.7 metres above internal floor level.

Reason: In the interests of the amenities of occupants of adjacent residential properties.

24 The development shall be constructed to a BRE Environmental Assessment Method (BREEAM) standard of 'excellent'. A Post Construction stage assessment shall be carried out and a Post Construction stage certificate shall be submitted to the Local Planning Authority within 3 months of occupation of the building. Should the development fail to achieve a BREEAM standard of 'excellent' a report shall be submitted for the written approval of the Local Planning Authority demonstrating what remedial measures should be undertaken to achieve a standard of 'excellent'. The approved remedial measures shall then be undertaken within a timescale to be approved in writing by the Local Planning Authority.

Reason: In the interests of achieving a sustainable development in accordance with the requirements of GP4a of the City of York Development Control Local plan (2005) and Policy CC2 of the Draft local Plan (2018).

25 The premises shall be used only as a Care Home within Use Class C2 and shall not be used for any other purpose, including any other purpose in Class C2 of the Schedule to the Town and Country Planning (Use Classes) Order 1987, as amended, or in any provision equivalent to that Class in any Statutory Instrument revoking and re-enacting that Order.

Reason: In order to allow a consideration of the impact of any changes on amenity.

8.0 INFORMATIVES:

Notes to Applicant

1. STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome:

Revised plans

2. NOTE: For noise details on hours of construction, deliveries, types of machinery to be used, use of quieter/silenced machinery, use of acoustic barriers, prefabrication off site etc, should be detailed within the CEMP. Where particularly noisy activities are expected to take place then details should be provided on how they intend to lessen

the impact i.e. by limiting especially noisy events to no more than 2 hours in duration. Details of any monitoring may also be required, in certain situation, including the location of positions, recording of results and identification of mitigation measures required.

For vibration details should be provided on any activities which may results in excessive vibration, e.g. piling, and details of monitoring to be carried out. Locations of monitoring positions should also be provided along with details of standards used for determining the acceptability of any vibration undertaken. In the event that excess vibration occurs then details should be provided on how the developer will deal with this, i.e. substitution of driven pile foundations with auger pile foundations. Ideally all monitoring results should be recorded and include what was found and mitigation measures employed (if any).

With respect to dust mitigation, measures may include, but would not be restricted to, on site wheel washing, restrictions on use of unmade roads, agreement on the routes to be used by construction traffic, restriction of stockpile size (also covering or spraying them to reduce possible dust), targeting sweeping of roads, minimisation of evaporative emissions and prompt clean up of liquid spills, prohibition of intentional on-site fires and avoidance of accidental ones, control of construction equipment emissions and proactive monitoring of dust. Further information on suitable measures can be found in the dust guidance note produced by the Institute of Air Quality Management, see <http://iaqm.co.uk/guidance/>. The CEMP must include a site specific risk assessment of dust impacts in line with the IAQM guidance note and include mitigation commensurate with the scale of the risks identified.

For lighting details should be provided on artificial lighting to be provided on site, along with details of measures which will be used to minimise impact, such as restrictions in hours of operation, location and angling of lighting.

In addition to the above the CEMP should provide a complaints procedure, so that in the event of any complaint from a member of the public about noise, dust, vibration or lighting the site manager has a clear understanding of how to respond to complaints received. The procedure should detail how a contact number will be advertised to the public, what will happen once a complaint had been received (i.e. investigation), any monitoring to be carried out, how they intend to update the complainant, and what will happen in the event that the complaint is not resolved. Written records of any complaints received and actions taken should be kept and details forwarded to the Local Authority every month during construction works by email to the following addresses

public.protection@york.gov.uk and planning.enforcement@york.gov.uk

3. INFORMATIVE:

The developer's attention is drawn to the various requirements for the control of noise

on construction sites laid down in the Control of Pollution Act 1974. In order to ensure that residents are not adversely affected by air pollution and noise, the following guidance should be adhered to, failure to do so could result in formal action being taken under the Control of Pollution Act 1974:

(a) All demolition and construction works and ancillary operations, including deliveries to and despatch from the site shall be confined to the following hours:

Monday to Friday 08.00 to 18.00

Saturday 09.00 to 13.00

Not at all on Sundays and Bank Holidays.

(b) The work shall be carried out in such a manner so as to comply with the general recommendations of British Standards BS 5228: Part 1: 1997, a code of practice for "Noise and Vibration Control on Construction and Open Sites" and in particular Section 10 of Part 1 of the code entitled "Control of noise and vibration".

(c) All plant and machinery to be operated, sited and maintained in order to minimise disturbance. All items of machinery powered by internal combustion engines must be properly silenced and/or fitted with effective and well-maintained mufflers in accordance with manufacturer's instructions.

(d) The best practicable means, as defined by Section 72 of the Control of Pollution Act 1974, shall be employed at all times, in order to minimise noise emissions.

(e) All reasonable measures shall be employed in order to control and minimise dust emissions, including sheeting of vehicles and use of water for dust suppression.

(f) There shall be no bonfires on the site

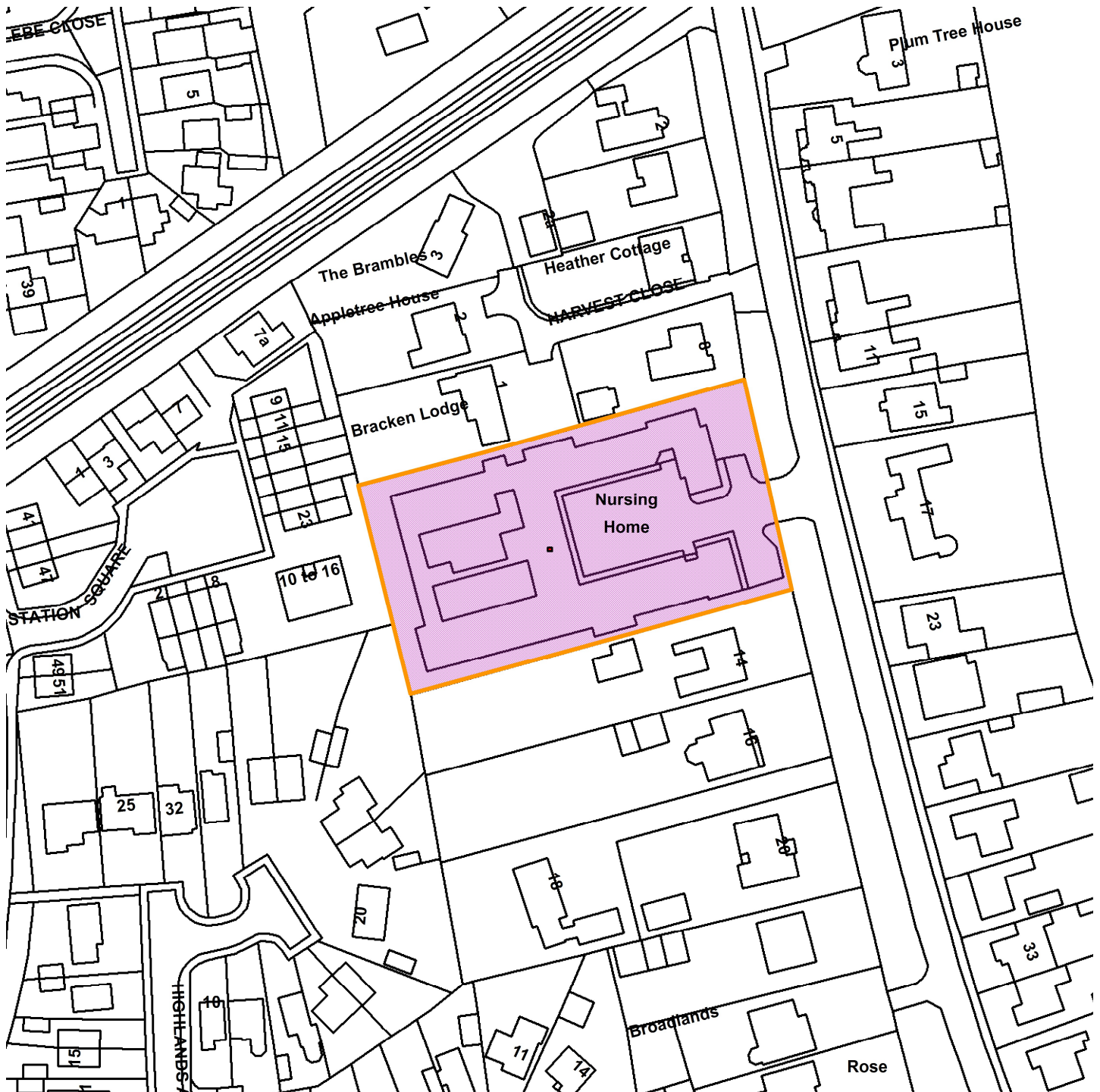
Contact details:

Case Officer: Tim Goodall

Tel No: 01904 551103

19/02044/FULM

Moorlands Nursing Home, 10-12 Moor Lane, Strensall YO32 5UQ



Scale : 1:1227

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Organisation	City of York Council
Department	Economy & Place
Comments	Site Location Plan
Date	11 August 2020
SLA Number	

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19/02044/FULM - Moorlands Nursing Home, 10 - 12
Moor Lane, Strensall, York

Erection of 62 bedroom care home with associated car parking and landscaping following the demolition of existing care home (resubmission of 18/02935/FULM)

Front Elevation 1



Front Elevation 2



Front Elevation 3



Internal Courtyard



Side Elevation



View from Moor Lane



Moor Lane looking North



View into Courtyard



View to North East of site



Page 50

View to North of site

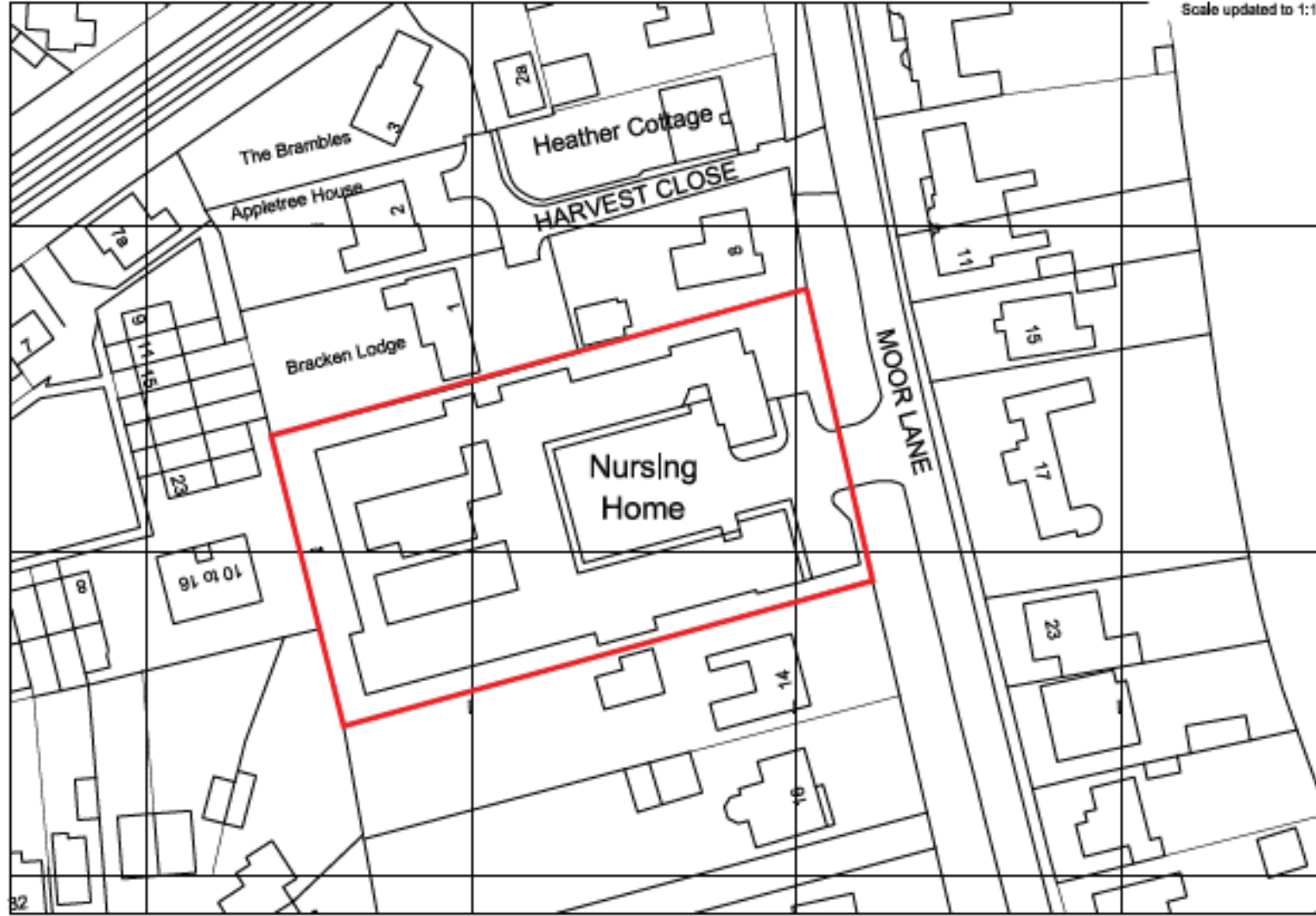


Page 51

Site Location Plan

Rev.	Date	By/Chk
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B	11.01.18	PLANNING JW

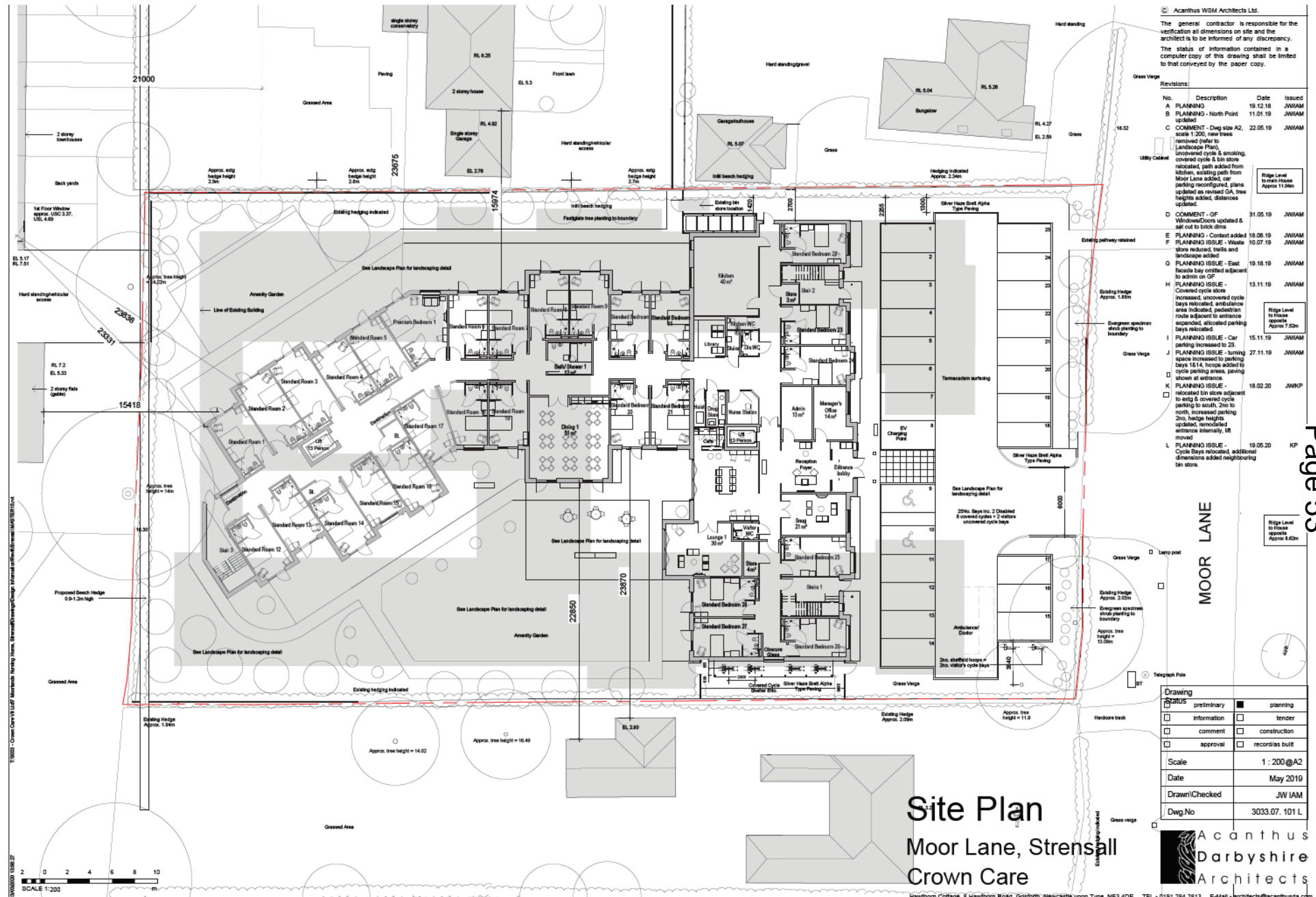
Scale updated to 1:1250 & to A4



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Proposed Site Plan



Site Plan
 Moor Lane, Strensall
 Crown Care



Proposed Elevations (1)

1. East Elevation
1:200

2. North Elevation
1:200

3. West Elevation (See South West Elevation for more information)
1:200

Material Schedule

1. Grey/Red 'Rivius' type roof tile
2. Black UPVC type fascia board
3. Buff/red multi facing brickwork with artstone string course/soldier course
4. Grey UPVC 'sash' style window frames with 'Aristone' type heads & cills
5. Grey UPVC framed glazed doors, some w. side panel with opening light
6. Coloured composite/timber door & frame
7. Black UPVC half round gutter & circular down pipe in 'cast iron' effect.
8. Cast stone type entrance canopy/portico
9. Grey UPVC rooflight
10. Cast Stone Type feature with Crown Care logo engraved
11. White/off white render
12. Timber clad waste store

Location Plan
1:2000

Elevations Sheet 1
Moor Lane, Strensall
Crown Care

The general contractor is responsible for the verification of all dimensions on site and the architect is to be informed of any discrepancy.
The status of information contained in a computer copy of this drawing shall be limited to that conveyed by the paper copy.

Revisions:

No.	Description	Date	Issued
A	PLANNING ISSUE - Elevation titles updated	19.12.19	JW/AM
B	PLANNING ISSUE - Elevation titles updated	11.01.19	JW/AM
C	PLANNING ISSUE - Street elevation broken down, mass & height reduced; 'sash' window opening course, context & landscaping, etc. added; stone, stone, stone added to West Elevation added; RWPs, south facing terrace location & material key updated; CP rooms to have direct access to gardens with doors and gates; S&T adjusted	10.06.19	JW/AM
D	PLANNING ISSUE - asymmetric roof to front elevation omitted	10.07.19	JW/AM
E	PLANNING ISSUE - East facade bay omitted adjacent to admin on CP	19.10.19	JW/AM
F	PLANNING ISSUE - Outline course at eaves level on main gables. Flat roof added to East EL moved to gables	09.09.19	JW/AM
G	PLANNING ISSUE - tin stone and cycle painting allocated, slope to roof updated and corners introduced to street facing block, previous height of elevations shown, lift & rooflights moved, base & hedge heights updated, render introduced on front elevation, terraces at roof level moved towards centre of building, gables (balustrading shown), 2 gabled bays to north now hipped. Entrance updated.	18.02.20	JW/MP
H	PLANNING ISSUE - Eaves on front elevation 1550 below 1500/19 Application, lift material & height updated.	21.02.20	JW/MP
I	PLANNING ISSUE - Previous application outline removed	21.02.20	JW/MP
J	PLANNING ISSUE - Crown Care logo note added to Premium Suite 2 location	28.06.20	MP

Drawing Status

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<input type="checkbox"/> information	<input type="checkbox"/> tender
<input type="checkbox"/> comment	<input type="checkbox"/> construction
<input type="checkbox"/> approval	<input type="checkbox"/> records built

Scale @ A3 As indicated
Date Nov 2018
Drawn/Checked JW IAM
Dwg No 3033.07.140 J



Elevations Sheet 1
Moor Lane, Strensall
Crown Care

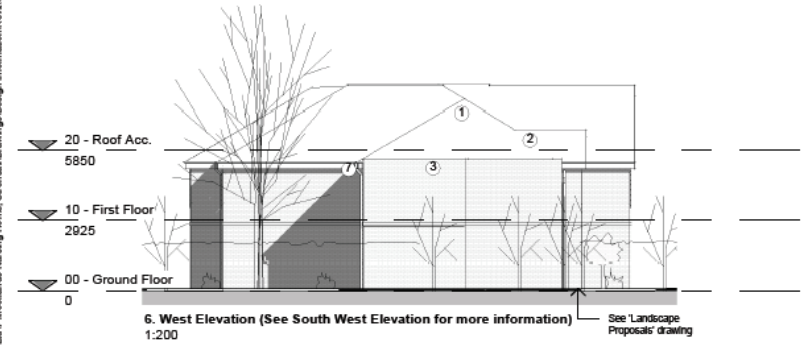


Proposed Elevations (2)

The general contractor is responsible for the verification of dimensions on site and the architect is to be informed of any discrepancy.
The status of information contained in a computer copy of this drawing shall be limited to that conveyed by the paper copy.

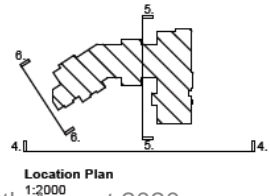
Revisions:

No.	Description	Date	Issued
A	PLANNING	18.12.18	JWAM
B	PLANNING - Elevation lines updated	11.01.19	JWAM
C	PLANNING ISSUE - Street elevation broken down, mass & height reduced, bays, 'artstone' string course, cornice & landscaping, bin store, shadows, more detail added to West Elevation added. Paving, south facing terrace location & material key updated. GP rooms to have direct access to outdoors with doors and opening light adjacent.	18.06.19	JWAM
D	PLANNING ISSUE - Terrace reduced at roof level	10.07.19	JWAM
E	PLANNING ISSUE - Asymptotic roof & red brick roof omitted to front elevation, terraces & landscape added to north side.	10.07.19	JWAM
F	PLANNING ISSUE - Bin store and cycle parking relocated, steps to roof updated and doors introduced to street facing block, previous height of elevations shown, lift & rooflights moved, see & facade heights updated, render introduced on front elevation, terrace at roof level moved towards centre of building, glass balustrading shown, 2 gabled bays to north now flipped. Entrance updated.	18.02.20	JWKP
G	PLANNING ISSUE - Eaves on front elevation 1350 below 1002019 Application, lift material & height updated.	21.02.20	JWKP
H	PLANNING ISSUE - Previous application outline removed.	21.02.20	JWKP



Material Schedule

- Grey/Red 'Rivius' type roof tile
- Black UPVC type fascia board
- Buff/red multi facing brickwork with artstone string course/soldier course
- Grey UPVC 'sash' style window frames with 'Artstone' type heads & cills
- Grey UPVC framed glazed doors, some w. side panel with opening light
- Coloured composite/timber door & frame
- Black UPVC half round gutter & circular down pipe in 'cast iron' effect.
- Cast stone type entrance canopy/porch
- Grey UPVC rooflight
- Cast Stone Type feature with Crown Care logo engraved
- White/off white render
- Timber clad waste store



Elevations Sheet 2

Moor Lane, Strensall Crown Care

Drawing Status

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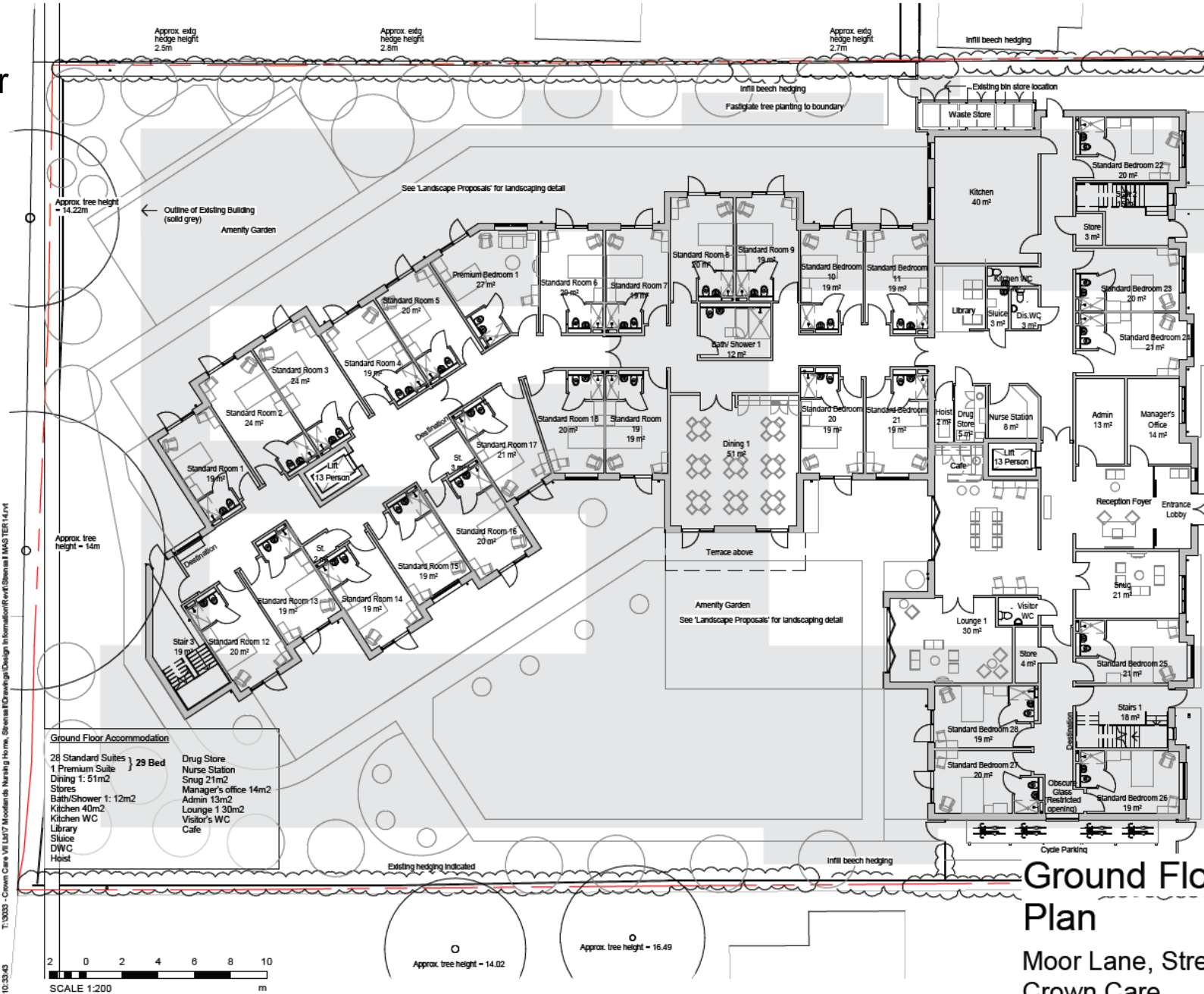
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Date Nov 2018
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Dwg.No 3033.07.141 H



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Proposed Ground Floor

The general contractor is responsible for the verification all dimensions on site and the architect is to be held responsible for any discrepancies.
The status of information contained in a computer copy of this drawing shall be limited to that conveyed by the paper copy.

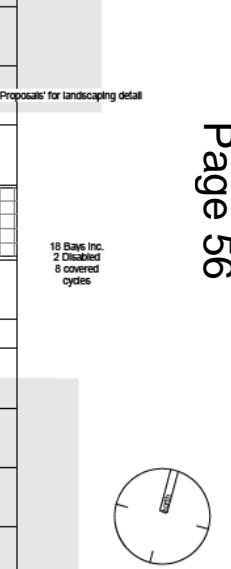


Ground Floor Accommodation

28 Standard Suites } 29 Bed	Drug Store
1 Premium Suite	Nurse Station
Dining 1: 51m ²	Snug 21m ²
Stores	Manager's office 14m ²
Bath/Shower 1: 12m ²	Admin 13m ²
Kitchen 40m ²	Lounge 1 30m ²
Kitchen WC	Visitor's WC
Library	Cafe
Suicide	
D/WC	
Heist	

Revisions:

No.	Description	Date	Issued
A	PLANNING - North Point updated	19.12.19	JWAM
B	PLANNING - North Point updated	11.01.19	JWAM
C	PLANNING - floor elevation height reduced, bedrooms 14 & 15 brought in, 16 pushed out, stores, dining, IT location, ensuite layout, front elevation & canopy changed. Suite removed (No. beds was) Premium Room, Kitchen & circulation windows removed. Library/Dis/WC & shop reconfigured. Manager's increased, lounge, lobby & admin increased. Garment & previous building footprint & landscape note added.	18.06.19	JWAM
D	PLANNING - Content added, lounge/visitor VIC updated	10.07.19	JWAM
E	PLANNING ISSUE - Restricted opening note added to circulation windows on front block	19.10.19	JWAM
F	PLANNING ISSUE - East facade bay limited adjacent to admin on site	18.02.20	JWAM
G	PLANNING ISSUE - relocated bin store adjacent to entry & covered cycle parking to south, 2no to north, increased parking 2no, hedge height updated, new entrance internally, lift moved		



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City of York Council Area Planning Sub Committee Meeting - 20th August 2020



Ground Floor Plan
Moor Lane, Strensall
Crown Care

Drawing Status	
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<input type="checkbox"/> information	<input type="checkbox"/> tender
<input type="checkbox"/> comment	<input type="checkbox"/> construction
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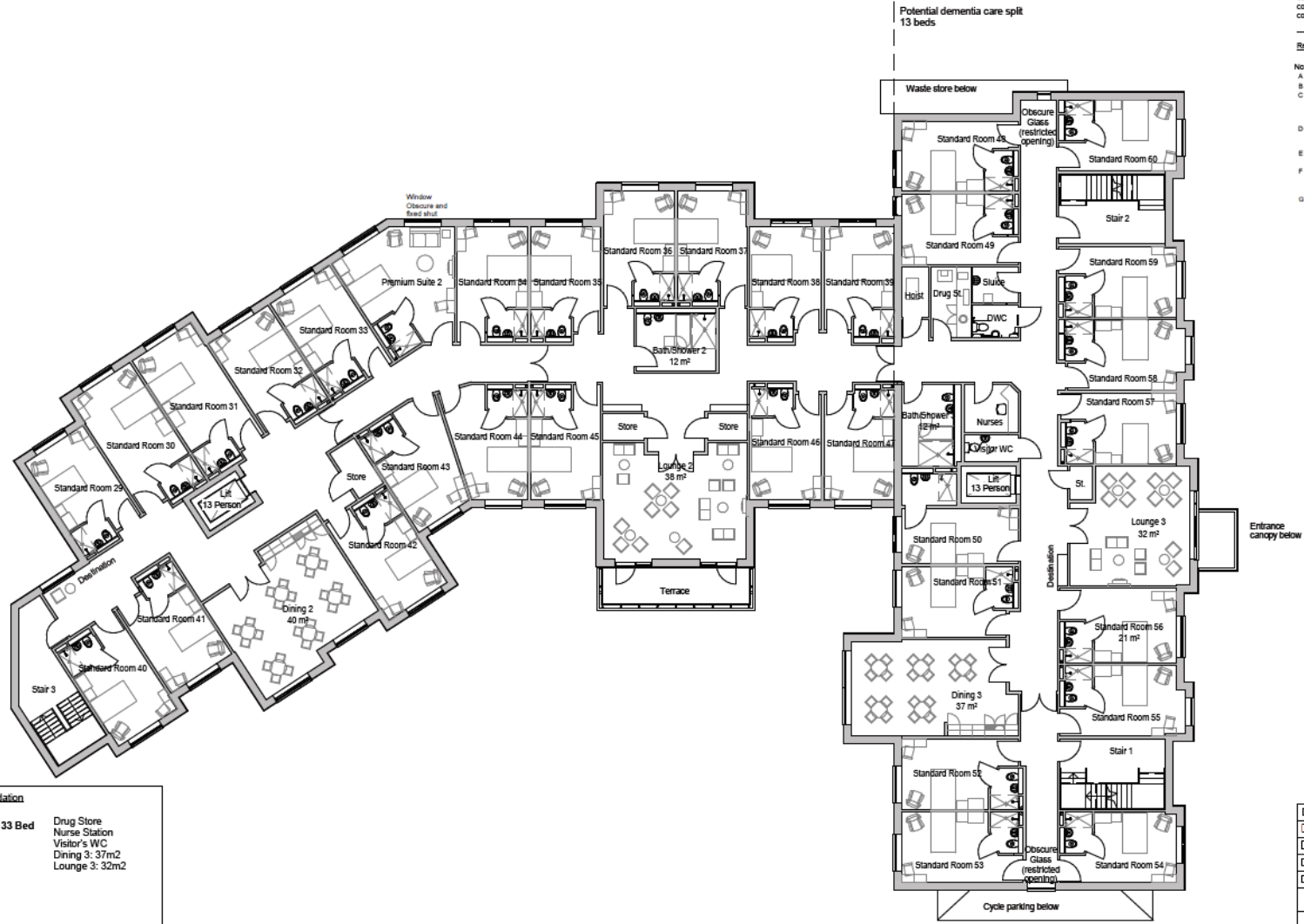


Proposed First Floor

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First Floor Accommodation	
32 Standard Suites	} 33 Bed
1 Premium Suite	
Dining 2: 40m ²	Drug Store
Stores	Nurse Station
Lounge 2: 38m ²	Visitor's WC
Bath/Showers: 12m ²	Dining 3: 37m ²
Sluice	Lounge 3: 32m ²
DWC	
Hoist	



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The status of information contained in a computer copy of this drawing shall be limited to that conveyed by the paper copy.

Revisions:			
No.	Description	Date	Issued
A	PLANNING	19.12.18	JWAM
B	PLANNING - North Front updated	11.01.19	JWAM
C	PLANNING - Lounge & Dining, terrace, stores, dementia split moved, hoist updated, terrace omitted.	18.08.19	JWAM
D	PLANNING ISSUE - Restricted opening note added to circulation windows on front block.	10.07.19	JWAM
E	PLANNING ISSUE - East facade bay omitted adjacent to admin on GP.	19.10.19	JWAM
F	PLANNING ISSUE - restricted bin and cycle covered areas, lift moved, glass balustrade to terrace shown.	18.02.20	JWAM
G	PLANNING ISSUE - Obscure Window rule added to Premium Suite 2 Bedroom.	25.06.20	JWAM

Drawing Status			
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<input type="checkbox"/>	information	<input type="checkbox"/>	tender
<input type="checkbox"/>	comment	<input type="checkbox"/>	construction
<input type="checkbox"/>	approval	<input type="checkbox"/>	records built
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Date		Nov 2018	
Drawn/Checked		JW IAM	
Dwg.No		3033.07. 131 G	

First Floor Plan Moor Lane, Strensall Crown Care



Healtham Cottage, 9 Healtham Road, Gosforth, Newcastle upon Tyne, NE3 4DE. TEL - 0191 284 2813. E-Mail - architects@acanthusda.com

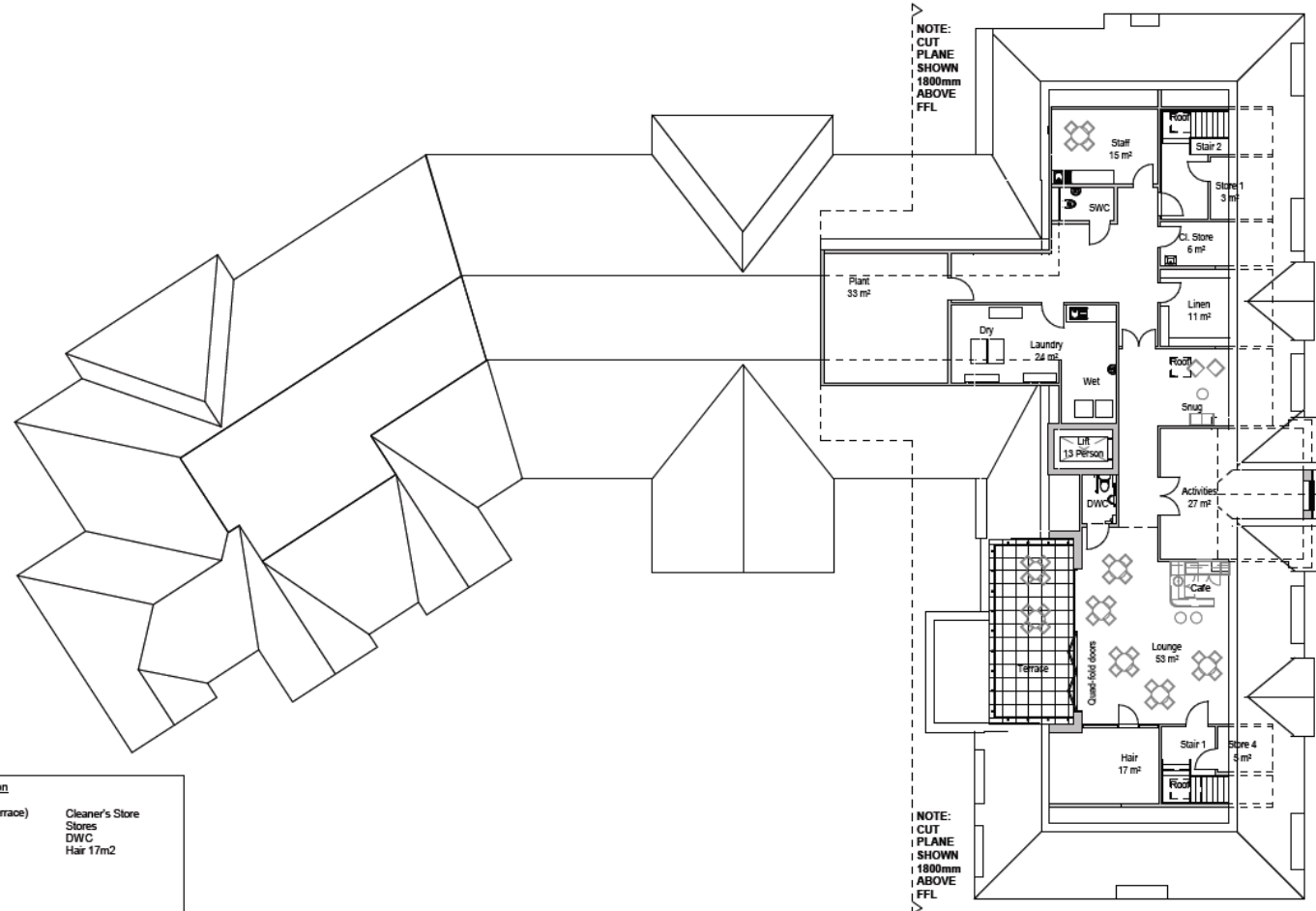


Roof Plan & Second Floor Plan

The general contractor is responsible for the verification all dimensions on site and the architect is to be informed of any discrepancy.
The status of information contained in a computer copy of this drawing shall be limited to that conveyed by the paper copy.

Revisions:

No.	Description	Date	Issued
A	PLANNING	10.12.18	JWAM
B	PLANNING - North Point updated	11.01.19	JWAM
C	PLANNING - Adapted room locations, adjusted terrace, gym removed, stairs added, escape distance & roof design revised	18.06.19	JWAM
D	PLANNING ISSUE - Stair 2 bay at roof level	10.07.19	JWAM
E	PLANNING ISSUE - East terrace bay limited adjacent to admin on GP	19.10.19	JWAM
F	PLANNING ISSUE - plan updated following change in roof slope and introduction of dormers to sheet facing back, fill moved, terrace moved towards centre of building, glass balustrade shown	18.02.20	JWKP
G	PLANNING ISSUE - Eaves on front elevation 1350 below 10/2019 Application, 98 material & height updated	21.02.20	JWKP



Roof Accommodation	
Lounge 53m2 (& Terrace)	Cleaner's Store
Activities 28m2	Stairs
Snug	DWC
Plant 32m2	Hair 17m2
Staff WC 16m2	
Laundry 24m2	
Linen 11m2	

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Roof Accommodation
Moor Lane, Strensall
Crown Care

Drawing Status	
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<input type="checkbox"/> information	<input type="checkbox"/> tender
<input type="checkbox"/> comment	<input type="checkbox"/> construction
<input type="checkbox"/> approval	<input type="checkbox"/> records built
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Date Nov 2018	
Drawn/Checked JW IAM	
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COMMITTEE REPORT

Date: 20 August 2020 **Ward:** Westfield
Team: West Area **Parish:** No Parish

Reference: 20/00494/FULM

Application at: 61A Gale Lane York YO24 3AD

For: Erection of 3 storey building to form 8no. apartments and 2no. semi-detached bungalows to the rear following demolition of existing bungalow with new access and associated landscaping.

By: Inglehurst Properties

Application Type: Major Full Application

Target Date: 9 June 2020

Recommendation: DELEGATE AUTHORITY TO OFFICERS TO DETERMINE AFTER EXPIRY OF CONSULTATION PERIOD SUBJECT TO NO NEW MATERIAL OBJECTIONS

1.0 PROPOSAL

1.1 The application site contains an existing bungalow located on the eastern side of Gale Lane. The site has an existing access onto Gale Lane and a front, side and rear garden. To the north of the site is a detached two storey dwelling that appears to be of nineteenth century construction. South and west of the site are mid twentieth century two storey dwellings, and they are predominantly semi detached. There are a number of larger buildings along Gale Lane including the Church of Our Lady on the southern corner of Gale Lane and Cornlands Road. There are examples of three storey blocks of flats along Gale Lane, to the north and further to the south.

1.2 Planning permission was granted in 2017 for the erection of a two storey building with 5 apartments on Gale Lane with a pair of bungalows to the rear. Following this, permission was granted for the erection of 7 apartments in a three storey building with the bungalows retained to the rear.

1.3 The current planning application seeks to increase the number of apartments to 8 in the main building by including an additional unit at (both on the ground floor) second floor level. There would be 6 one bedroom flats and 2 two bedroom flats accessed via a single entrance fronting onto Gale Lane. The parking would be to the rear, via an archway with one space per dwelling and an additional visitor space. Cycle parking and waste storage is proposed. The bungalows would be retained as before.

1.4 The apartment building would have a dual pitch roof with a front gable end projection. It would be a maximum of 10.25 metres high, up to 15.4 metres deep and 16.6 metres wide. The proposed materials would be concrete grey tiles for the roof and red brickwork for the walls with uPVC windows.

1.5 The bungalows would be semi-detached and 4.8 metres high at the ridge. The combined width of the bungalows would be 15.8 metres and they would be 9 metres deep. The materials would match the apartment building in terms of tile and brick.

1.6 Planning History

17/00555/FUL - Erection of 5no. apartments and 2no. semi-detached bungalows to rear following the demolition of existing building – Permitted 31.08.2017

19/00583/FULM - Erection of 5 one bedroom and 2 two bedroom apartments and 2no. two bedroom semi-detached bungalows following demolition of existing bungalow. – Permitted 02.08.2019

1.7 The application is presented to committee as it is major development (10 or more dwellings). It is noted that the previous planning permissions were granted by officers under delegated authority as they were of a reduced scale (7 and 9 units).

1.8 The application is currently subject to re-consultation of neighbours following the introduction of additional obscure glazing to upper floor side windows and also to the change of roof materials from clay tiles to concrete tiles. The consultation period ends on 21st August. The recommendation for this application therefore proposes that the final decision is delegated to officers following completion of the consultation period and subject to no new material objections being raised.

2.0 POLICY CONTEXT

National Planning Policy Framework

2.1 The revised National Planning Policy Framework (NPPF) 2019 was published on 19 February 2019 and sets out the government's planning policies for England

and how these are expected to be applied. The policies are material planning considerations.

2.2 The planning system should contribute to the achievement of sustainable development (Paragraph 7). To achieve sustainable development, the planning system has three overarching objectives; economic, social and environmental objectives. Paragraph 14 advises that at the heart of the Framework there is a presumption in favour of sustainable development.

2.3 Section 5 of the NPPF supports the government's objective of significantly boosting the supply of homes. Paragraph 60 states that the size, type, tenure of housing needed for different groups in the community should be assessed and reflected in planning policies.

2.4 Section 12 sets out that the creation of high quality buildings and places is fundamental to what planning and development process should achieve. Section 14 of the NPPF seeks to meet the challenge of climate change.

Publication Draft Local Plan 2018

2.5 The Publication Draft City of York Local Plan 2018 ('2018 Draft Plan') was submitted for examination on 25 May 2018. In accordance with paragraph 48 of the NPPF the Draft Plan policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012. (NB: Under transitional arrangements plans submitted for examination before 24 January 2019 will be assessed against the 2012 NPPF).

2.6 The evidence base underpinning the 2018 Draft Plan is capable of being a material consideration in the determination of planning applications.

2.7 Relevant Policies

H2 Density of Residential Development

D1 Placemaking

GI4 Trees and Hedgerows

ENV2 Managing Environmental Quality

ENV5 Sustainable Drainage

T1 Sustainable Access

CC1 Renewable and Low Carbon Energy Generation and Storage

CC2 Sustainable Design and Construction of New Development

2005 Development Control Local Plan

2.8 The Development Control Local Plan (DCLP) was approved for development management purposes in April 2005. Whilst the DCLP does not form part of the statutory development plan, its policies are considered to be capable of being material considerations in the determination of planning applications where policies relevant to the application are consistent with those in the NPPF, albeit with very limited weight.

3.0 CONSULTATIONS

INTERNAL

Design Conservation and Sustainable Development (Landscape)

3.1. Comments on previous application: “there are no trees worthy of a TPO within the boundary of the application site. The larger trees to the rear are beyond the site boundary.”

Highways Network Management

3.2 No objection following submission of vehicle tracking for parking spaces. Conditions recommended.

Public Protection

3.3 Conditions recommended for land contamination and electric vehicle charging. Informative recommended for construction hours.

Waste Management

3.4 Location of the bin store is acceptable.

EXTERNAL

Ainsty Drainage Board

3.5 Condition recommended for drainage strategy.

Yorkshire Water

3.6 No objection subject to compliance with submitted drawing.

North Yorkshire Police

3.7 The overall design and layout of the proposed scheme is considered acceptable.

4.0 REPRESENTATIONS

4.1 The application has been advertised by neighbour notification, press and site notice. Letters of objection were received from the occupier of 61 Gale Lane (to north of site) and from the owner of 63 Gale Lane (to south of site).

4.2 The following concerns were raised:

- loss of light from three storey building
- window overlooking garden
- presume side facing windows are obscured
- loss of light to garden and property

4.3 Following concerns from officers and neighbours, revised plans were received showing obscure glazing to side windows above ground floor level and a change of roofing material to concrete tiles. These revised plans were subject to re-consultation. No comments have yet been received.

5.0 APPRAISAL

5.1 Key Issues

- principle of development
- housing need
- impact on the streetscene
- impact on amenity and living conditions of adjacent occupiers
- highways and parking
- drainage
- sustainability

PRINCIPLE OF DEVELOPMENT

5.2 The application site is an existing dwelling and has an existing permission for the erection of 9 residential units that could be implemented. The principle of residential development on the site is therefore established.

HOUSING NEED

5.3 Policy DP2 of the Draft Plan states that development will address the housing need of York's current and future population. Policy DP3 states development should contribute to a sustainable, balanced community through provision of an appropriate range of housing. Policy H2 suggests a density of 40 units per hectare for new residential development in the York suburban area. It also states higher densities can be achieved with 400 metres of high frequency public transport corridors, such as bus route 4, which is close to the application site. The proposed development will provide an additional two residential units compared to the previously approved scheme and is therefore considered to be in general accordance with the aforementioned policies and also Section 5 of the NPPF which seeks to boost housing supply. Policy H10 For suburban sites that have a maximum combined gross floorspace of more than 1,000sqm, affordable housing contributions are required. The gross floorspace of the application is under this threshold, and therefore contributions will not be sought.

IMPACT ON THE STREETSCENE

5.4 Paragraph 127 of the NPPF sets out government guidelines for good design. Policy D1 of the 2018 Draft Plan states that proposals will be supported where they improve poor existing urban and natural environments, enhance York's special qualities and better reveal the significances of the historic environment. Development proposals that fail to take account of York's special qualities, fail to make a positive design contribution to the city, or cause damage to the character and quality of an area will be refused.

5.5 The proposed design for a three storey residential block is considered to be acceptable in this location given there are several examples of similar development on Gale Lane. The location is on a relatively busy through road and close to Acomb district centre. The building is of a similar height to the adjoining dwelling to the north west and is set back from the highway edge with amenity space provided. The proposed bungalows to the rear are very similar to those previously approved in 2017 and 2019. When considered against the previous approval, there is a modest increase of 0.25 metres in height and a marginal increase in the footprint of the building. There are some alterations to the front elevation in comparison to the previous approval such as the removal of a double height bay window and a change of roofing material from clay tiles to grey concrete tiles. However the main design of the proposal is very similar with the archway through to the car park at the rear. The change in roofing material is considered acceptable given that Gale Lane is very varied in this respect. The applicant has confirmed details of the materials in an attempt to remove pre-commencement conditions. The red brick and concrete tiles are considered acceptable. As such, the amended scheme is considered to be

similar to the previous approval and acceptable in line with policy D1 of the 2018 Draft Plan and paragraph 127 of the NPPF.

IMPACT ON NEIGHBOURS' LIVING CONDITIONS

5.6 The NPPF states that developments should create places with a high standard of amenity for all existing and future users. It goes on to state that decisions should avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development. Policies D1 and ENV2 of the 2018 Draft Plan also consider amenity.

5.7 The proposed development will not result in a change of use, but increasing the number of dwellings on the site from 1 to 10. Permission already exists for 9 units on the site, including the pair of bungalows at the rear. The main issues are the increase in height in terms of loss of light and privacy and also the increase in comings and goings. In terms of light, the three storey building will respect the existing building line of Gale Lane and will be detached from neighbouring buildings. The additional storey will be set back from the highway and due to its orientation and the distance from neighbouring dwellings there is not considered to be an unacceptable impact in terms of the loss of light. The height of the building is 0.25 metres above the previous approval. This would not make a material difference to the level of light to the occupiers of neighbouring dwellings. Concerns have been raised by the occupiers of no 61 to the north in terms of privacy. As a result the north facing windows above ground floor level are to be obscure glazed. This arrangement is considered to be acceptable on amenity grounds.

5.8 In terms of comings and goings, the addition generated by two extra residential units is not considered to give rise to significant adverse impacts for local residents. Gale Lane is a relatively busy thoroughfare and local residents would therefore expect a certain level of background noise from traffic.

HIGHWAYS

5.9 Policy T1 of the 2018 Draft Plan states that to provide safe, suitable and attractive access, development proposals will be required to demonstrate there is safe and appropriate access to the adjacent adopted highway. Development proposals should also create safe and secure layouts for motorised vehicles (including public transport vehicles), cyclists, pedestrians that minimise conflict.

5.10 The proposal includes a parking space for each unit, accessed via a new entrance off Gale Lane. A visitor space is also proposed. Secure cycle spaces for 10

bicycles are proposed. Revisions to the scheme have been submitted that altered the layout of the car park to overcome the concerns of the Highways Officer of cars not being able to exit the site in forward gear given the location on Gale Lane and in close proximity to the mini roundabout. The parking levels are considered acceptable for the location. The proposal is subject to conditions considered to be appropriate on highways grounds.

DRAINAGE

5.11 The application site lies in flood zone 1. Surface and foul water drainage is proposed via the mains sewer, although the drainage strategy submitted notes this is based on an assumption that infiltration will not work in this location. The strategy goes onto note that “site percolation testing will be undertaken prior to any development to confirm this.” It is therefore considered reasonable that a pre-commencement condition be applied to cover the drainage strategy as per SUDs hierarchy to establish if a soakaway would work in this situation.

SUSTAINABILITY

5.12 The application site lies in relatively sustainable location being approximately 400 metres from Acomb district centre. The site is also close to local shops and a bus stop on Gale Lane as well as facilities on Cornlands Road including a GP surgery, pharmacy and leisure centre. Secure cycle parking is also proposed. The applicant is aware of the requirements under Policies CC1 and CC2 of the 2018 Draft Plan with regard to climate change adaption. These will be imposed by planning condition to ensure the draft policies are complied with.

6.0 CONCLUSION

6.1 The proposed development departs from the previous approved scheme slightly increasing the scale of the scheme, however these are modest increases, such as 0.25 metre increase in height to the proposed apartment block. The proposal would result in an increase in the number of units approved under permission 19/00583/FULM by one, thereby according with the national and local policies for boosting housing supply. It is noted that the revised plans including the change in roofing material and the introduction of further obscure glass to windows is subject to re-consultation which ends on 21st August. No comments have yet been received and it is considered the introduction of obscure glazing overcomes the concerns raised in terms of privacy to neighbours. When considered against national and local planning policies, the proposed development is considered to be acceptable subject to the following conditions.

7.0 RECOMMENDATION: DELEGATE AUTHORITY TO OFFICERS TO DETERMINE AFTER EXPIRY OF CONSULTATION PERIOD SUBJECT TO NO NEW MATERIAL OBJECTIONS

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans and other submitted details:-

20010-130-P01 received 29 July 2020

20010-131-P02 received 29 July 2020

20010-230-P00 received 29 July 2020

19014 105 P05 received 13 May 2020

20010_110-P00 received 9 March 2020

20010_111-P00 received 9 March 2020

20010_112-P00 received 9 March 2020

20010_132-P00 received 9 March 2020

20010_210-P00 received 9 March 2020

20010_001-P00 received 9 March 2020

20010_108-P00 received 31 July 2020

Materials Schedule dated 21st July 2020

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 DRAIN1 Drainage details to be agreed

4 LC4 Land contamination - unexpected contam

5 The development shall not be occupied until there has been submitted and approved in writing by the Local Planning Authority a detailed landscaping scheme which shall illustrate the number, species, height and position of trees and shrubs. This scheme shall be implemented within a period of six months of the completion of the development. Any trees or plants which within a period of five years from the completion of the development die, are removed or become seriously damaged or

diseased shall be replaced in the next planting season with others of a similar size and species, unless alternatives are agreed in writing by the Local Planning Authority.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species within the site in the interests of the character and appearance of the area.

6 No part of the site shall come into use until the turning areas have been provided in accordance with the approved plans. Thereafter the turning areas shall be retained free of all obstructions and used solely for the intended purpose.

Reason: To enable vehicles to enter and leave the site in a forward gear thereby ensuring the safe and free passage of traffic on the public highway.

7 Prior to the development coming into use 2.0 x 2.0m sight lines, free of all obstructions which exceed the height of the adjacent footway by more than 0.6m, shall be provided both sides of the junction of any access with the footway, and shall thereafter be so maintained.

Reason: In the interests of pedestrian safety.

8 The building shall not be occupied until the areas shown on the approved plans for parking and manoeuvring of vehicles (and cycles, if shown) have been constructed and laid out in accordance with the approved plans, and thereafter such areas shall be retained solely for such purposes.

Reason: In the interests of highway safety.

9 The development shall not be occupied until all existing vehicular crossings not shown as being retained on the approved plans have been removed by reinstating the footway to match adjacent levels.

Reason: In the interests of good management of the highway and road safety.

10 Prior to the development coming into use, all areas used by vehicles shall be surfaced, sealed and positively drained within the site, in accordance with the approved plans.

Reason: To prevent the egress of water and loose material onto the public highway.

11 The development shall not come into use until the junction with the public highway has been constructed in accordance with the approved plans.

Reason: In the interests of road safety.

12 Unless otherwise agreed in writing with the Local Planning Authority, the boundary treatment detailed on the Proposed Site Plan drawing number 19-014-105-P03, shall be carried out in its entirety prior to the development hereby approved being first brought into use.

Reason: In the interests of the amenities of neighbouring occupiers.

13 Notwithstanding the provisions of Article 3 of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order revoking or re-enacting that Order), development of the type described in Classes A, B, C and E of Schedule 2 Part 1 of that Order shall not be erected or constructed.

Reason: In the interests of the amenities of the adjoining residents the Local Planning Authority considers that it should exercise control over any future extensions or alterations which, without this condition, may have been carried out as "permitted development" under the above classes of the Town and Country Planning (General Permitted Development) Order 2015.

14 Prior to works starting on site a dilapidation survey of the highways adjoining the site shall be jointly undertaken with the Council and the results of which shall be agreed in writing with the Local Planning Authority.

Reason: In the interests of the safety and good management of the public highway the details of which must be recorded prior to the access to the site by any construction vehicle.

8.0 INFORMATIVES: Notes to Applicant

1. STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome:

Amended plans

2. INFORMATIVE:

The developer's attention is drawn to the various requirements for the control of noise on construction sites laid down in the Control of Pollution Act 1974. In order to ensure that residents are not adversely affected by air pollution and noise, the following guidance should be adhered to, failure to do so could result in formal action being taken under the Control of Pollution Act 1974:

(a) All demolition and construction works and ancillary operations, including

deliveries to and despatch from the site shall be confined to the following hours:

Monday to Friday 08.00 to 18.00

Saturday 09.00 to 13.00

Not at all on Sundays and Bank Holidays.

(b) The work shall be carried out in such a manner so as to comply with the general recommendations of British Standards BS 5228: Part 1: 1997, a code of practice for "Noise and Vibration Control on Construction and Open Sites" and in particular Section 10 of Part 1 of the code entitled "Control of noise and vibration".

(c) All plant and machinery to be operated, sited and maintained in order to minimise disturbance. All items of machinery powered by internal combustion engines must be properly silenced and/or fitted with effective and well-maintained mufflers in accordance with manufacturer's instructions.

(d) The best practicable means, as defined by Section 72 of the Control of Pollution Act 1974, shall be employed at all times, in order to minimise noise emissions.

(e) All reasonable measures shall be employed in order to control and minimise dust emissions, including sheeting of vehicles and use of water for dust suppression.

(f) There shall be no bonfires on the site

Contact details:

Case Officer: Tim Goodall

Tel No: 01904 551103

61A Gale Lane, YO24 3AD

20/00494/FULM



Scale : 1:1227

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Organisation	City of York Council
Department	Economy & Place
Comments	Site Location Plan
Date	11 August 2020
SLA Number	

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20/00494/FULM - 61A Gale Lane, York

Erection of 3 storey building to form 8no. apartments and 2no. semi-detached bungalows to the rear following demolition of existing bungalow with new access and associated landscaping.

View from Street



Page 74

View from Street (2)



Street View (3)



Existing Garden from Street



View between 61
Gale Lane and Site



Side Elevation of 61
Gale Lane



View from West



Site Location Plan

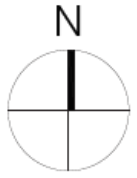
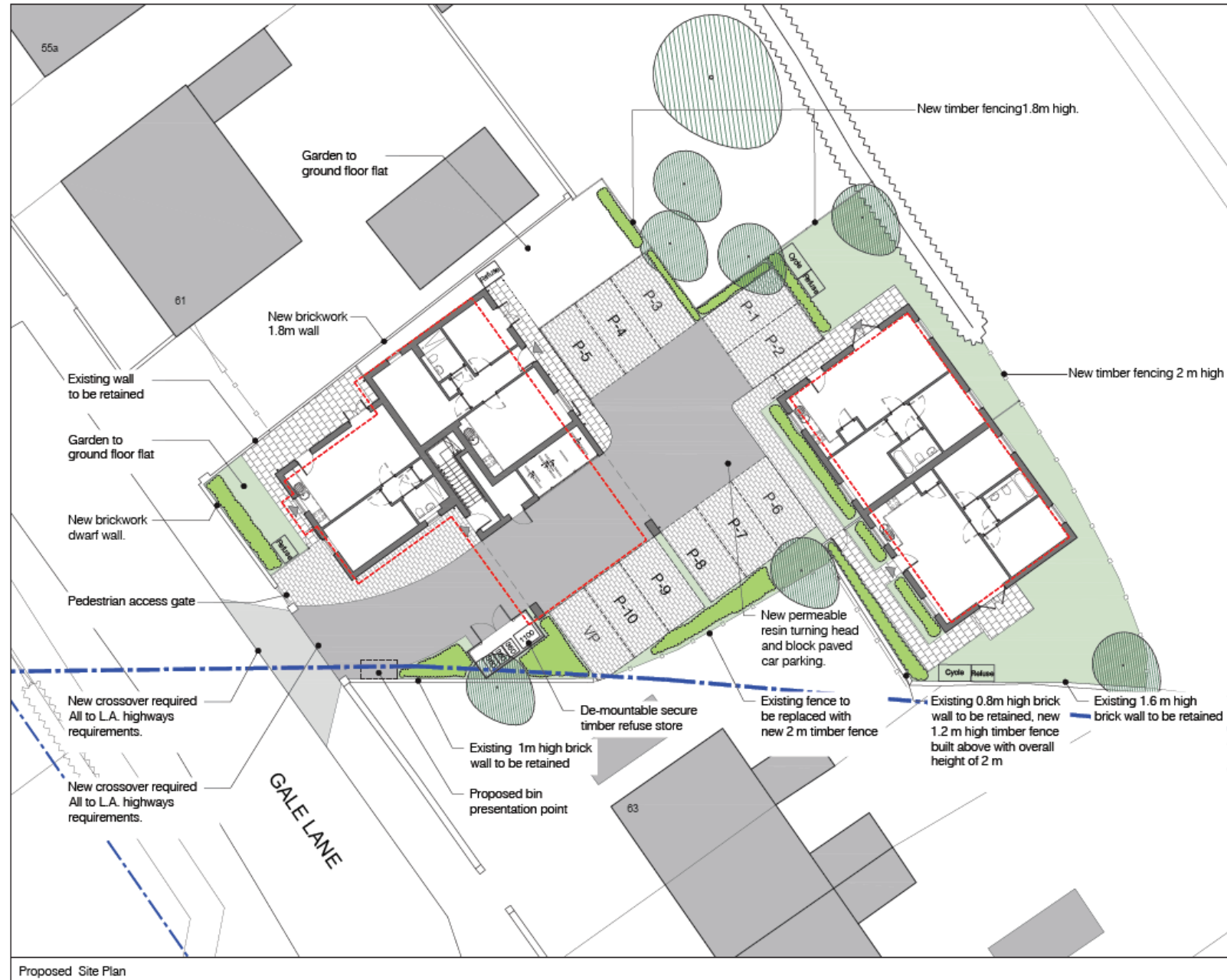


Site Location Plan



revision	date	description		
<p>Vincent & Brown Studio 12, Middlethorpe Business Park Six Hills Lane, Bishopthorpe, York, YO23 2BP t: 01904 200841 e: info@vincentandbrown.com</p>				
<p>project: Proposed residential development 59/61a Gale Lane, Acomb, York, YO24 3AD</p>				
<p>drawing: Location Plan</p>				
<p>scale: 1:1250 @ A4</p>				
job number	date	scale	drawing number	revision
20010	31.01.2020	Planning	001	P00

Proposed Site Plan



--- Outline of approved building

NO	DATE	DESCRIPTION
P05	12.05.20	cycle parking to GP flat structure external cycle store
P06	22.01.2020	Building footprint adjusted and landscaping revised
P07	23.07.19	Recycling treatment area updated
P08	23.07.19	Parking amended to take comments from planning
P09	17.05.19	Proposed scheme revised following planners comments

Vincent & Brown

Studio 13, Millthorpe Works Station Road
 Sherburn Lane, Millthorpe, York, YO20 2BP
 T: 01904 730561 e: info@vincentandbrown.com

project
 59/61a Gale Lane, Acomb

drawing
 Proposed Site Plan

scale
 1:200 @ A3

job number	date	stage	drawing number	revision
19014	16.01.2020	Planning	105	P05



Existing Street Elevation & Proposed Street Elevation



Do not scale from this drawing
 Drawing to be read in conjunction with the specification document
 Contractor to check all dimensions on site and notify the architect of any discrepancies prior to construction of the works



Vincent & Brown
 Studio 12, Middlehope Business Park
 5th & 6th Lane, 850 Highgate, York, YO24 3BD
 T: 01904 730941 e: staff@vincentandbrown.com

project
 Proposed residential development
 55/56 1a Gale Lane, Acomb, York, YO24 3AD

drawing
 Proposed Street Elevations

scale
 1:200 @ A3

job number date issue drawing number revision
 20010 31.01.2020 Planning 140 P00

Proposed Elevations (1)



Do not scale from this drawing
Drawing to be read in conjunction with the specifications document
Contractor to check all dimensions on site and notify the architect of any discrepancies prior to commencement of the work.



REF	DATE	DESCRIPTION
FD1	31.01.2020	Finalised

Vincent & Brown
Studio 51, Middlehope Business Park
51a Gale Lane, Scarborough, York, YO11 2BD
T: 01904 730961 E: info@vincentandbrown.com

PROJECT
Proposed residential development
53/51a Gale Lane, Acomb, York, YO24 3AD

DRAWING
Proposed Elevations
Apartments - Sheet 1

SCALE
1:100 @ A3

JOB NUMBER	DATE	SCALE	DRAWING NUMBER	VERSION
20010	31.01.2020	Planning	130	FD1

Proposed Elevations (2)



Proposed North East Elevation



Proposed North West Elevation

Do not scale from this drawing
Drawing to be read in conjunction with the specification document
Contractor to check all dimensions on site and notify the architect of any discrepancies prior to commencement of the works

revision	date	description
P02	28.07.20	Window size revised to suit planners comments
P01	23.07.20	Roofline size revised

Vincent & Brown
Studio 12, Middlethorpe Business Park
59/61a Gale Lane, Acomb, York, YO24 3AD
t: 01904 700943 e: studio@vincentandbrown.com

project
Proposed residential development
59/61a Gale Lane, Acomb, York, YO24 3AD

drawing
Proposed Elevations
Apartments - Sheet 2

scale
1:100 @ A3

job number	date	status	drawing number	revision
20010	31.01.2020	Planning	131	P02

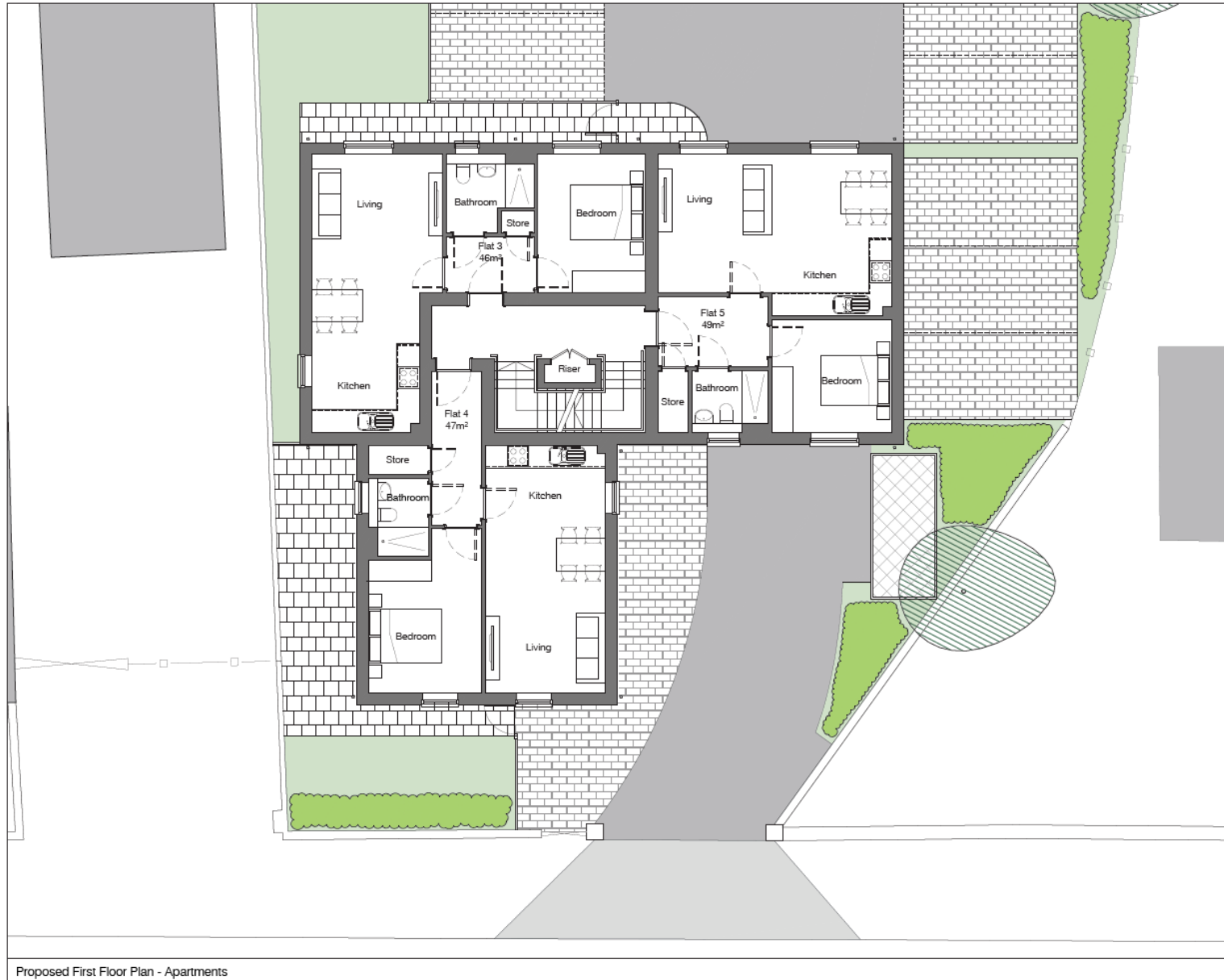


Proposed Ground Floor



revision	date	description
<p>Vincent & Brown Studio 11, Middlethorpe Business Park 5th-6th Lane, Bishopton, York, YO21 2BD t: 01904 789961 e: studio@vincentandbrown.com</p>		
<p>project Proposed residential development 55/57a Gale Lane, Acomb, York, YO24 3AD</p>		
<p>drawing Proposed Ground Floor Plan Apartments</p>		
<p>scale 1:100 @ A3</p>		
job number	date	sheet
20010	31.01.2020	Planning 110
drawing number	revision	
110	P00	

Proposed First Floor



Do not scale from this drawing
 Drawing to be read in conjunction with the specification document
 Contractor to check all dimensions on site and notify the architect of any discrepancies prior to commencement of the works



revision	date	description
<p>Vincent & Brown Studio 12, Middlethorpe Business Park 5th-8th Lane, Middlethorpe, York, YO22 3BQ t: 01904 722943 e: studio@vincentandbrown.com</p>		
<p>project Proposed residential development 59/61a Gale Lane, Acomb, York, YO24 3AD</p>		
<p>drawing Proposed First Floor Plan Apartments</p>		
<p>scale 1:100 @ A3</p>		
0	5	
02010	31.01.2020	Planning 111 P00



Proposed Second Floor

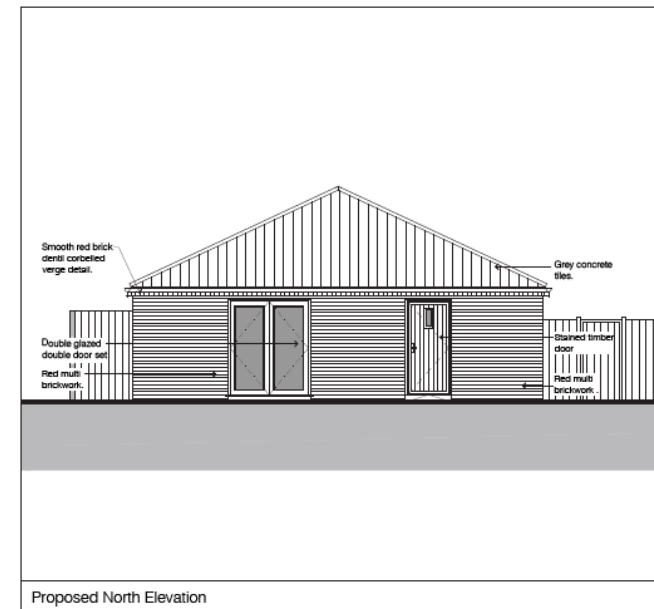
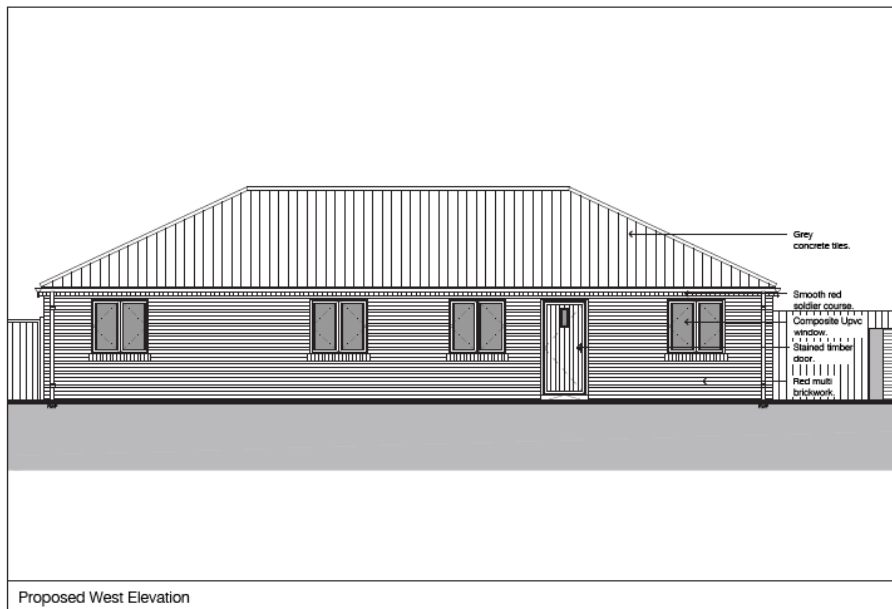
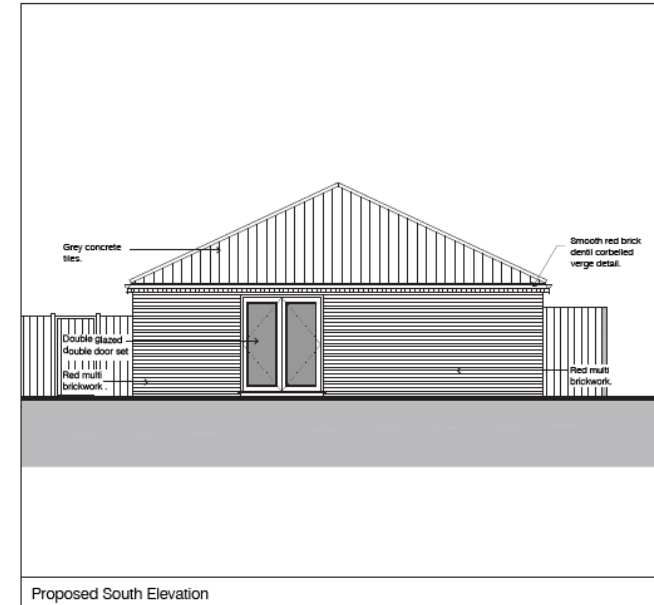
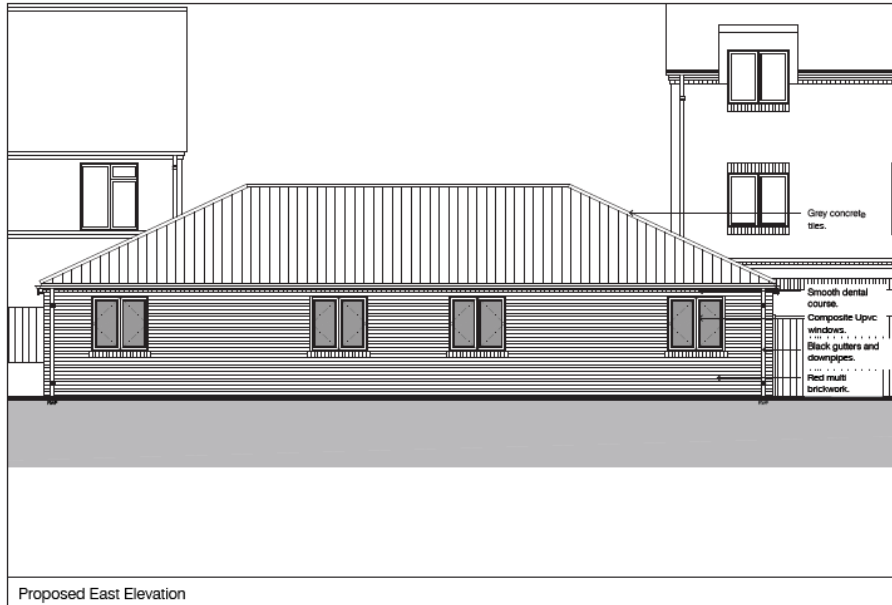


Do not scale from this drawing
 Drawing to be read in conjunction with the specification document
 Contractor to check all dimensions on site and notify design team of any discrepancies prior to commencement of the works



revision	date	description
<p>Vincent & Brown Suite 12, Middlethorpe Business Park Six Balk Lane, Bishopthorpe, York, YO23 2BD t: 01904 700041 e: studio@vincentandbrown.com</p>		
<p>project Proposed residential development 59/61a Gale Lane, Acomb, York, YO24 3AD</p>		
<p>drawing Proposed Second Floor Plan Apartments</p>		
<p>scale 1:100 @ A3</p>		
job number	date	status
20010	31.01.2020	Planning
drawing number	revision	
112	P00	

Proposed Elevations, Bungalow

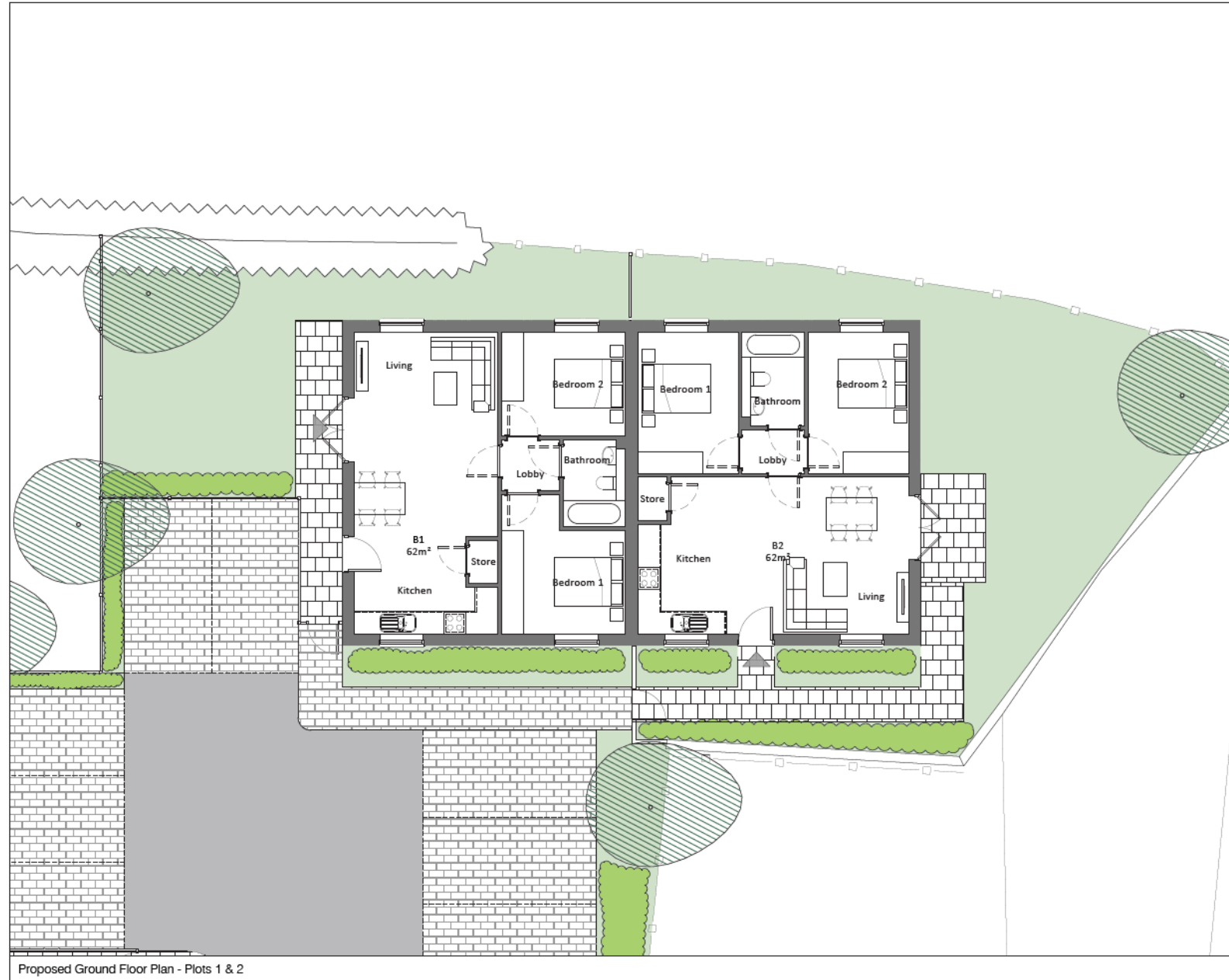


Do not scale from this drawing
Drawing to be read in conjunction with the specification document
Contractor to check all dimensions on site and notify co-architect of any discrepancies prior to commencement of the works



P21 23.07.20 Roof lines now revised
 revision date description
Vincent & Brown
 Studio 13, Middleforge Business Park
 Sim Ball Lane, Bishopthorpe, York, YO23 2BD
 t: 01904 700945 e: studio@vincentandbrown.com
 project
 Proposed residential development
 59/61a Gale Lane, Acomb, York, YO24 3AD
 drawing
 Proposed Elevations
 Plots 1 and 2
 scale
 1:100 @ A3
 drawing number
 20010 31.01.2020 Planning 230 P00

Proposed Floors Plans, Bungalows



Vincent & Brown	
Studio 12, Middlethorpe Business Park 59/61a Gate Lane, Acomb, York, YO24 3AD t: 0204 70041 e: studio@vincentandbrown.com	
Project Proposed residential development 59/61a Gate Lane, Acomb, York, YO24 3AD	
Drawing Proposed Ground Floor Plan Plots 1 and 2	
Scale 1:100 @ A3	
Job number	20010
Date	31.01.2020
Status	Planning
Drawing number	210
Revision	P00

COMMITTEE REPORT

Date: 20 August 2020 **Ward:** Dringhouses And Woodthorpe
Team: West Area **Parish:** Dringhouses/Woodthorpe Planning Panel

Reference: 19/02729/OUT
Application at: Site to the rear of 5 Cherry Lane York
For: Outline application for the erection of 5no. detached dwellings with means of access
By: Crossways Commercial Estates Ltd
Application Type: Outline Application
Target Date: 10 March 2020
Recommendation: Approve subject to Section 106 Agreement

1.0 PROPOSAL

1.1 The proposal is for the construction of 5 detached dwellings on a 0.9Ha site. The application is in outline with all matters, excluding access, reserved. Access will be taken from a new junction where Cherry Lane meets St Edwards Close.

The site

1.2 The site is an undeveloped field adjacent to the Knavesmire. It is accessed via Cherry Lane which leads off Tadcaster Road, past the Holiday Inn Hotel, and down to a small car park serving the Knavesmire. The bottom half of the lane becomes very rural in character being narrow with high hedges on the Southern side. These hedges form part of a Site of Importance for Nature Conservation (SINC) with intermittent TPO trees within the hedgerow. To the North is a residential area formed of large individual detached properties in large plots.

1.3 To the East the site abuts the Knavesmire while to the South it shares a boundary with the racing stables. An access from the stables to the Knavesmire runs close to this boundary. The paddock to the South of the boundary is also a

SINC. A vehicle maintenance shed has recently been granted permission (19/02730/FUL) within the stables site and close to the application boundary.

1.4 The Knavesmire falls within Flood Zone 2 and the Green Belt.

2.0 POLICY CONTEXT

2.1 Emerging Local Plan (Submission draft 2018)

DP2 Sustainable Development

DP3 Sustainable Communities

SS1 Delivering Sustainable Growth for York

H2 Density of Residential Development

H10 Affordable housing

D1 Placemaking

D2 Landscape and Setting

D6 Archaeology

GI1 Green Infrastructure

GI3 Green Infrastructure Network

GI4 Trees and Hedgerows

CC1 Renewable and Low Carbon Energy Generation and Storage

CC2 Sustainable Design and Construction of New Development

ENV2 Managing Environmental Quality

ENV5 Sustainable Drainage

T1 Sustainable Access

DM1 Infrastructure and Developer Contributions

2.2 Development Control Local Plan (including 4th set of changes) 2005

H4A Housing Windfalls

GP1 Design

HE10 Archaeology

HE11 Trees and landscape

L1c Open space

3.0 CONSULTATIONS

Internal consultations

Design, Conservation and Sustainable Development (Archaeology)

3.1 The site is just outside Dringhouses Area of Archaeological Importance. The site has not been disturbed by development and has been in agricultural use since at least the medieval period. It is possible that late prehistoric-Roman evidence of land use (e.g. field systems) may exist on the site. Further archaeological evaluation of the site is required and can be secured via planning condition.

Public Protection

3.2 Conditions recommended for electric vehicle charging, construction environmental management plan (CEMP) and land contamination.

The applicant has submitted a noise impact assessment to demonstrate the impact of the surrounding commercial activities on the proposed residential dwellings. The report concludes that standard double glazed units within the development will be sufficient to ensure that internal noise levels can be achieved.

Design, Conservation and Sustainable Development (Landscape)

3.3 The site is located within district level green infrastructure corridor 10 'Knavesmire/Hob Moor', historic environment character 74 'York Racecourse and the Knavesmire' in the York Historic Environment Characterisation Project; and lies immediately adjacent to character 75 'Dringhouses and Tadcaster Road'. It sits on the edge of Micklegate stray. The strays of York are a key character element of the City of York's Landscape and Setting as identified in the Heritage Topic Paper, September 2014.

The photomontages provided with the application are helpful. Retention of the existing tree cover on the perimeter of the site is possible and will help to mitigate the visual impact of the proposal.

Landscape buffers are to be provided on the South and East boundaries outside of the curtilages of the dwellings as well as a landscaped strip adjacent to the hedge SINC on Cherry Lane. Details of how these are to be managed should be confirmed.

Design, Conservation and Sustainable Development (Ecology)

3.4 The hedgerow along Cherry Lane would be outside the curtilage of the proposed houses, separated by the private driveway. The proposal to safeguard the integrity of the retained hedgerow into the future through the production and implementation of a Habitat Management Plan is supported.

The site is part of the Knavesmire/Hob Moor District Green Corridor. Green corridors are not fixed boundaries but are a consensus of where green infrastructure assets occur. It is a significant multifunctional corridor for not only wildlife but historic landscape and recreation as well. The proposed development will contribute to the erosion this Green Corridor, although not significantly affect its integrity.

Conditions recommended.

Highways Network Management

3.5 Comments on the initial submission raised concern about priority arrangements and visibility lines. Revised plans have been submitted as agreed with Highways officers and conditions recommended to secure detail. It was noted that parking provision for the development is high.

Planning Policy

3.6 The site has been appraised for its development potential through the Local Plan process. The western part of the application site was included as an allocation (H2b) in the Publication Draft Local Plan (2014; Plan subsequently withdrawn); the eastern part of the site was considered unsuitable for development as it provides a buffer to the SINC hedgerow on Cherry Lane and any development coming forward would need to provide a suitable buffer to this hedgerow and to the Knavesmire Stables Meadow SINC. It was also considered that development of the additional land proposed to the east of H2b would have an adverse impact on the character of Cherry Lane and the open aspect the site currently provides to the Knavesmire. H2b

itself was subsequently removed from the Plan at Preferred Sites stage (2016) due to the site's restricted narrow access and the impact of mature hedgerows and trees on the site's developable area.

At the Local Plan Preferred Sites consultation, a report taken to Local Plan Working Group (July 2017) identified a number of potential changes to the emerging Plan, taking into account further evidence and consultation responses received. This report included options for boundary changes to proposed site allocations and potential new sites which conformed with the Council's approach to site selection.

Following Officers' consideration of further technical work submitted, the report included Land at Cherry Lane (H2b) as a potential allocation. Technical officer assessment considered that the former allocation could be suitable for development if existing trees and hedgerows could be retained and if it could be developed in a way which retained the rural character of Cherry Lane. Officers were clear that an alternative boundary (including land to the east of H2b) would not be supported. However, Members resolved that site H2b should not be included as an allocation in the emerging Plan, as it was not required in order to meet the Plan's agreed level of housing need. The site is not currently allocated in the emerging Plan.

Housing Policy

3.7 For residential sites of 0.5ha an affordable housing contribution is required in accordance with the submitted Local Plan Policy H10, Affordable Housing (NPPF reference, paragraph 63 and Glossary, "Major Development"). The site to which this application relates passes this threshold at 0.9ha.

An affordable housing contribution would be required which may be on site or off site as a commuted sum, dependent on the number of homes confirmed at Reserved Matters stage. The site lies in the "York Urban Area" identified by the Local Plan Density Zones map. The affordable housing contribution for the site would be £31,547.98 per dwelling secured within a S106 agreement.

Education

3.8 An education contribution is required for this application for 7 places across all 3 sectors totalling £141,159 based on current place cost multipliers and an

assumption that the houses in this development will have an average of 5 bedrooms each. Infrastructure projects would be at Dringhouses Primary, or a nearby school in same planning area, Millthorpe secondary school and Early Years provision within 1.5km. This will be secured in the form of a formula within a S106 agreement.

Further update – see paragraph 5.30 below.

External responses

Yorkshire Water

3.9 Condition recommended for separate systems of foul and surface water drainage.

Ainsty Internal Drainage Board

3.10 The Board notes that the applicant intends to use an existing surface water drain. This drain appears to, in turn, connect into the Knavesmire Culvert. The Board's understanding of the Knavesmire Culvert is that it then discharges into the River Ouse further south and does not enter a Board maintained watercourse, or any ordinary watercourse in the Board's district. On this basis, the Board would have no comments to make. However, should these details change the Board would wish to be re-consulted.

4.0 REPRESENTATIONS

4.1 Twelve letters of objection (four of which have been logged twice resulting in eight original letters) have been received in relation to the application including one signed by residents of St Edwards Close and individual letters from a number of those residents. The issues raised include:

- The houses will not be affordable
- Loss of a green space and consequences for the climate emergency
- Changes to the character of the area as a result of a number of small infill sites being developed
- Increase in traffic
- Poor sight lines for traffic exiting the site on to Cherry Lane

- Impact on views from properties on Hunters Way
- The properties are inappropriate in design for Cherry Lane and St Edwards Close
- The site is a haven for wildlife
- Road safety concerns for school children using Cherry Lane
- Concern about flooding/ standing water in properties nearest Knavesmire
- The proposal takes development beyond the existing building line and closer to the Knavesmire
- Concern about surface water drainage flooding
- Concern about conflict between pedestrians and cars on Cherry Lane
- The site is not allocated in Local Plan
- In relation to the revised highways'
- layout, concern was raised about a lack of signage indicating routes to the racecourse and lack of a road safety audit

5.0 APPRAISAL

5.1 Main Issues

- principle of development;
- design;
- highways and parking;
- drainage;
- trees;
- ecological issues;
- sustainability;
- affordable housing.

LEGISLATIVE AND POLICY CONTEXT

5.2 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires determinations be made in accordance with the development plan unless material considerations indicate otherwise.

National Planning Policy Framework

5.3 The revised National Planning Policy Framework (NPPF) 2019 was published on 19 February 2019 and sets out the government's planning policies for England and

how these are expected to be applied. The NPPF is a material planning consideration in the determination of this application.

5.4 The planning system should contribute to the achievement of sustainable development (Paragraph 7). To achieve sustainable development, the planning system has three overarching objectives; economic, social and environmental objectives.

5.5 Paragraph 11 states planning decisions should apply a presumption in favour of sustainable development and that for decision taking this means where there are no relevant development plan policies, granting permission unless:

- i. the application of policies in the NPPF that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or
- ii. any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies of the NPPF taken as a whole.

Publication Draft Local Plan 2018

5.6 The Publication Draft City of York Local Plan 2018 ('2018 Draft Plan') was submitted for examination on 25 May 2018. Phase 1 of the hearings into the examination of the Local Plan took place in December 2019. In accordance with paragraph 48 of the NPPF the Draft Plan policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012. (NB: Under transitional arrangements plans submitted for examination before 24 January 2019 will be assessed against the 2012 NPPF).

It is considered that given the stage of preparation of the emerging plan, nature of objections and consistency with the NPPF that all the policies listed in para. 2.1 above should be given moderate weight except H10 which should be given limited weight.

5.7 The evidence base underpinning the 2018 Draft Plan is capable of being a material consideration in the determination of planning applications. The directly relevant evidence base documents for this application are :

- City of York Housing Needs Update (2019)
- Strategic Housing Land Availability Assessment and Appendices (2018)
- Strategic Housing Land Availability Assessment and Annexes (2017)
- City of York Strategic Housing Market Assessment Update (2017)
- City of York Council Strategic Housing Market Assessment (2016)
- City of York Council Strategic Housing Market Assessment Addendum (2016)

2005 Development Control Local Plan

5.8 The Development Control Local Plan (DCLP) was approved for development management purposes in April 2005. Whilst the DCLP does not form part of the statutory development plan, its policies are considered to be capable of being material considerations in the determination of planning applications where policies relevant to the application are consistent with those in the NPPF albeit with very limited weight.

PRINCIPLE OF DEVELOPMENT

5.9 The site has been considered through the site selection process however as a result of concerns about impacts on the hedgerow SINC, character of Cherry Lane and the open aspect of the Knavesmire has not been allocated. It has now come forward as a windfall greenfield site. Section 11 (Making effective use of land) of the NPPF encourages the use of previously developed land first and gives substantial weight to this but does not preclude the use of greenfield sites. Paragraph 11 of the NPPF lays out the presumption in favour of sustainable development. Paragraph 8 identifies that there are 3 objectives to achieving sustainable development –

economic, social and environmental. These are not criteria against which every decision can or should be judged (paragraph 9).

5.10 The area is predominantly residential in character up to the Knavesmire. The site is unallocated (other designations related to the hedgerow SINC and adjoining Stables Pastures SINC are covered below) and in a sustainable location with good public transport links to the city centre. The Council does not have a 5 Year Housing Land Supply and as such, provided the proposal is otherwise acceptable, the provision of residential development on the site is acceptable in principle.

DESIGN

5.11 The application is in outline with all matters except access being reserved. The indicative site plan shows five large detached properties with detached double garages all set in substantial gardens. Access comes in at the North West corner of the site and runs parallel to Cherry Lane along the northern edge of the site.

5.12 The site is approximately 0.9Ha in area. Policy H2 of the 2018 Draft Plan sets housing density levels for different zones within the city. The site would fall within a zone where a housing density of 50 units/Ha is suggested. The policy does however note that housing density should be informed by the local character of the area. The site sits within an area where housing density is low. St Edwards Close is characterised by large properties in significant sized plots. The character of Cherry Lane also changes from the junction with St Edwards Close to one of a more rural lane with hedgerows and glimpses of undeveloped land. As such it is considered that the proposed low density development is appropriate to the character of the area and complies with policy H2 of the 2018 Draft Plan as it preserves local character by retaining greenspace on site and responding to local character.

5.13 The properties indicated with the application are two storey with brick and slates or tiles being the proposed materials. These details are considered in keeping with the general character of the area and details could be agreed at reserved matters stage. Given the sensitivity of the site in views from the Knavesmire it is considered appropriate to condition that the dwellings are no more than 9m in height as this is the scale which has been assessed in photomontages provided of the development from the Knavesmire.

HIGHWAYS AND PARKING

5.14 Details of the new junction have been agreed to ensure safety for all road users. Cherry Lane becomes a single track road after the junction and is well used as access to the Knavesmire. There is no footpath on this section of the road which is used by vehicles, pedestrians and horse-riders. To address this safety issue the road has been realigned with a speed control table introduced. The detail of this will be controlled via condition.

5.15 As the proposed access road will only serve five dwellings and not provide any through route (already provided by Cherry Lane), the authority would not be looking to adopt the access road. The new junction and sufficient additional length to preserve visibility would be adopted but no further. This would also potentially support the future residents in managing access and parking on their road during racecourse events.

5.16 It is recognised that each property has a double garage and a minimum of 3 additional parking spaces which would be in excess of guidance parking levels. Paragraph 106 of the NPPF states that maximum parking standards should only be set where there is a clear and compelling justification that they are necessary for managing the local road network or for optimising the density of development. On this site it has already been noted that low density development is appropriate. The level of parking also reflects the scale of the properties and will help to ensure that visitor parking does not spill out on to Cherry Lane or St Edwards Close.

TREES AND LANDSCAPE

5.17 Properties on St Edwards Close are set back from the Knavesmire and, along with the site, give a sense of green space and tree cover which contributes to the attractive setting and character of Micklegate Stray. The tree cover is visible across the Stray and contributes therefore to its character and setting and provides local distinctiveness. Denser residential areas to the South of the application site are strongly defined and tightly form and give a harder edge to the stray.

5.18 The development is compatible with retention of the existing tree cover around the perimeter of the site and includes additional tree and shrub planting along the

southern boundary where vegetation is lacking. Supplementary planting is also proposed to the Knavesmire. The existing and proposed planting would help to mitigate the visual impact of the proposed development but there would still be a discernible presence of the solid buildings behind the trees. To this extent there would be some loss of the sense of openness within this stretch of green space.

5.19 Cherry Lane would retain its essential character due to the existing/ proposed vegetation and set back of the properties. The loss of openness on the other side of the hedge would be apparent particularly at the site entrance so there would be a slight change in visual amenity for those using Cherry Lane.

5.20 The proposal does not require the removal of any of the individually surveyed trees on site of which 3 are covered by TPO. The distance between existing trees and the proposed development is considered acceptable in terms of outlook and root protection. A short section of hedge (approximately 11.5m or 6%) will be removed to facilitate the site access. The hedgerow is a designated SINC and its integrity would be preserved into the future through the production and implementation of a Habitat Management Plan. The TPO hornbeam at the access point would be protected and a no-dig cellular system is proposed for the road construction at this point to protect tree roots.

5.21 Local Plan policy GI4 recognises the value of trees and hedgerows to biodiversity, the contribution they make to the quality of a development and how they can help it be assimilated in to the landscape. This development has been shown to benefit from retained tree and hedgerow cover and new planting which will provide screening of the development, enhance residential amenity and ecological benefits. As such the proposal is considered to comply with policy GI4.

ECOLOGICAL ISSUES

5.22 Policy GI2 requires that development avoids loss or significant harm to SINC. Given that the proposal is for removal of 6% of the hedgerow and that the SINC would benefit from a management plan going forwards it is considered that the proposal complies with policy GI2 in this matter.

5.23 The site to the south is also a SINC and is so designated for its neutral grassland. This site connects through to the Stray which is managed under a

Higher Level Stewardship for nature conservation. These areas and the site itself are all part of the Knavesmire/Hob Moor District Green Corridor. Policy GI3 seeks to maintain and enhance the integrity and management of York's green infrastructure network, including its green corridors and open spaces. Green Corridors are not fixed boundaries but are a consensus of where green infrastructure assets occur. It is a significant multifunctional corridor for not only wildlife but also for historic landscape and recreation. The proposed development is considered to contribute to the erosion of this Green Corridor, although not significantly affect its integrity. The new native species planting on the southern boundary will go some way to maintaining habitat connectivity with adjacent sites. For these reasons it is considered that the proposal will on balance maintain York's green infrastructure network and therefore complies with policy GI3.

AMENITY ISSUES

5.24 The layout shows large houses in significant plots. As such it is considered that amenity for residents will be acceptable and issues of overlooking between properties can be overcome at the detailed design stage. There are no existing properties in such close proximity that the proposal would impact on their amenity through overlooking, overshadowing or over-dominance.

5.25 The site sits to the North of the stables for the racecourse. A storage shed has recently been given permission within the paddock adjacent to the site. A noise assessment has been undertaken to consider issues of noise impact from the stables on future residents. The report concludes that standard double glazed units within the development will be sufficient to ensure that internal noise levels can be achieved. The application does include the installation of a fence along the southern boundary. Provided this is installed then noise from the external sources should not have any adverse impact on the future residents of the dwellings.

5.26 There is some conflict between the requirement to provide a close-boarded fence on the southern boundary to provide noise insulation and concern raised by the landscape officer. In landscape terms the preference would be for a Paladin fence to allow views through to the landscaping. In addition, solid fences can sometimes lead to an 'out of sight, out of mind' mentality which might encourage tipping of garden waste over the fence. However the fence is 1.8m tall and it seems unlikely that residents would manage to tip waste over this. The green buffer would

remain open to the South of the site and the fence would give good sound insulation and privacy. On balance it is considered that the fence provides a wider benefit than Paladin fencing and is the preferred option.

DRAINAGE

5.27 Update to be provided at committee.

SUSTAINABILITY

5.28 A condition has been recommended to ensure the proposal complies with policies CC1 and CC2 of the 2018 Draft Plan. The developer suggests that sustainable measures such as electric vehicle charging, high levels of insulation and reduced water consumption will be included in the scheme at reserved matters stage. The proposed planning condition will ensure that appropriate additional measures are included.

AFFORDABLE HOUSING

5.29 The NPPF defines major development for housing as 10 or more houses or where the site area is 0.5Ha or more. At 0.9Ha the site is a major development and, in accordance with policy H10 of the 2018 Draft Plan, an affordable housing contribution is due. This may be on site provision or off site as a commuted sum. Sub-clause iii of policy HW10 allows for sites of 2-15 homes to provide on off-site contribution in accordance with a formula rather than on-site provision of affordable housing. For 5-10 houses in the York urban area the contribution per dwelling is £31,547.98 per dwelling. This would be secured in a S106 agreement.

EDUCATION

5.30 Whilst colleagues in Education have recommended that an education contribution of £141,159 is payable, their latest guidance 'Education Supplementary Planning Guidance – June 2019 Update' only requires such contributions from developments of 10 or more houses. Given that the proposal is for 5 houses (albeit the site size is greater than 0.5Ha and therefore it falls within the NPPF definition of major development), there is therefore considered no policy requirement for such a

contribution and the recommendation does not include this as an obligation on the development.

6.0 CONCLUSION

6.1 The site has been considered through the Local Plan process and the majority of the eastern side of the site has been assessed through that process as not suitable for development as a result of a likely adverse impact on the SINC hedgerow, character of Cherry Lane and open aspect to the Knavesmire. There is however an identified need for housing in York and the proposal would provide 5 dwellings as well as contributions towards affordable housing.

6.2 The scheme as submitted has been assessed as maintaining the integrity of the hedgerow SINC. A condition requiring a management plan will ensure its future maintenance. Evidence submitted with the application also indicates that the properties will have a minimal impact on views from the Knavesmire as a result of the scale of the dwellings and low density form of development. The layout has been designed to move the development away from Cherry Lane, the Stables SINC and the Knavesmire and issues raised during the Local Plan process are thereby considered to be overcome.

6.3 Given the benefits of the scheme in providing housing and affordable housing contributions while preserving the adjacent SINCS, it is considered that the minimal harm to the open setting the site provides to the Knavesmire is outweighed. The proposal is considered to meet relevant policy in the NPPF and emerging Local Plan and is recommended for approval subject to conditions and a S106 agreement securing affordable housing.

COMMITTEE TO VISIT

6.0 RECOMMENDATION: Approve subject to:

(i) Section 106 Agreement to secure the following obligation –
- Affordable housing (contribution of £31,547.98 per dwelling)

1 OUT1 Approval of Reserved Matters

2 Fully detailed drawings illustrating all of the following details shall be submitted to and approved in writing by the Local Planning Authority prior to the

commencement of building works, and the development shall be carried out in accordance with such details:

Details to be submitted: appearance, landscaping, layout and scale of the proposed development to be carried out, including a schedule of all external materials to be used.

Reason: In order that the Local Planning Authority may be satisfied as to the details of the development and to comply with The Town and Country Planning (Development Management Procedure) (England) Order 2015.

3 The development hereby permitted shall be carried out in accordance with the following plans and other submitted details:-

Location plan Y81.948.01

Indicative highway layout LCL-AWP-00-GF-DR-0003-P3

Junction layout LCL-AWP-00-GF-DR-0006-P1

Vehicle tracking LCL-AWP-00-GF-DR-0007-P1

Illustrative site layout Y81.948.03

Illustrative streetscape Y81.948.05

Illustrative sections Y81.948.06

Detailed landscape proposal 50176-DR-LAN-101 C

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

4 Any application for the approval of reserved matters submitted in accordance with condition 2 shall generally conform with the information, location and details set out in the illustrative site plan Y81:948.03 and the detailed landscape proposal plan 50176-DR-LAN-101 particularly in regard to:

Provision of a landscape buffer of approximately 4.2m in depth on the South and East boundary of the site

Reason: In the interests of clarity.

5 The total number of two storey residential dwellings shall not exceed 5 and those houses shall be no greater than 9m in height.

Reason: In the interests of visual amenity and the character of the area.

6 Notwithstanding any proposed materials specified on the approved drawings or in the application form submitted with the application, samples of the external

materials to be used shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of above ground works. The development shall be carried out using the approved materials.

Note: Because of limited storage space at our offices it would be appreciated if sample materials could be made available for inspection at the site. Please make it clear in your approval of details application when the materials will be available for inspection and where they are located.

Reason: So as to achieve a visually cohesive appearance.

7 Details of all means of enclosure to the site boundaries shall be submitted to and approved in writing by the Local Planning Authority before the commencement of above ground works and the approved means of enclosure shall be provided in accordance with the approved details before the development is occupied.

Reason: In the interests of the visual amenities of the area and the amenities of neighbouring properties.

8 A programme of post-determination archaeological evaluation is required on this site.

The archaeological scheme comprises 3-5 stages of work. Each stage shall be completed and approved by the Local Planning Authority (LPA) before it can be approved/discharged.

A) No archaeological evaluation (geophysical survey or trenching) or development shall commence until a written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. The WSI should conform to standards set by LPA and the Chartered Institute for Archaeologists.

B) The site investigation and post investigation assessment shall be completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (A) and the provision made for analysis, publication and dissemination of results and archive deposition will be secured. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.

C) A copy of a report on the evaluation and an assessment of the impact of the proposed development on any of the archaeological remains identified in the evaluation shall be deposited with City of York Historic Environment Record to allow public dissemination of results within 6 weeks of completion or such other period as may be agreed in writing with the Local Planning Authority.

D) Where archaeological features and deposits are identified proposals for the preservation in-situ, or for the investigation, recording and recovery of archaeological remains and the publishing of findings shall be submitted as an amendment to the original WSI. It should be understood that there shall be presumption in favour of preservation in-situ wherever feasible.

E) No development shall take place until:

- details in D have been approved and implemented on site
- provision has been made for analysis, dissemination of results and archive deposition has been secured
- a copy of a report on the archaeological works detailed in Part D should be deposited with City of York Historic Environment Record within 3 months of completion or such other period as may be agreed in writing with the Local Planning Authority.

This condition is imposed in accordance with Section 16 of NPPF.

Reason: The site lies within an area of archaeological interest. An investigation is required to identify the presence and significance of archaeological features and deposits and ensure that archaeological features and deposits are either recorded or, if of national importance, preserved in situ.

9 The development should incorporate sufficient capacity within the electricity distribution board for one dedicated radial AC single phase connection to allow the future addition of an Electric Vehicle Recharge Point (minimum 32A) within the garage space (or parking area) of each property as required. The applicant shall identify the proposed location for a future Electric Vehicle Recharge Point within the development curtilage and ensure that any necessary trunking/ducting is in place to enable cables to be run to the specified location.

Reason: To ensure future electric vehicle charge points can be easily added to the the property in line with the NPPF and CYC's Low Emission Strategy.

10 Development shall not begin until details of foul and surface water drainage works have been submitted to and approved in writing by the Local Planning Authority, and carried out in accordance with these approved details.

Reason: So that the Local Planning Authority may be satisfied with these details for the proper drainage of the site.

11 A Hedgerow Management Plan for Cherry Lane SINC (SINC site reference: SE54 08-YK) shall be submitted to, and be approved in writing by, the local planning

authority prior to occupation of the development. The content of the plan shall be produced in accordance with BS 42020:2013 and include the recommendations set out within Section 7 Mitigation and Enhancement of the Ecology Survey Report by Enviroscope Consulting Ltd dated May 2019 as already submitted with the planning application and agreed in principle with the local planning authority prior to determination.

Reason: To ensure the long term protection and management of the designated Site of Importance for Nature Conservation in line with 2018 Draft Plan policy GI2.

12 No development shall take place (including demolition, ground works, vegetation clearance) until a construction environmental management plan (CEMP: Biodiversity) has been submitted to and approved in writing by the local planning authority. The CEMP (Biodiversity) shall be produced in accordance with BS 42020:2013 and include the recommendations set out within Section 7 Mitigation and Enhancement of the Ecology Survey Report by Enviroscope Consulting Ltd dated May 2019 as already submitted with the planning application and agreed in principle with the local planning authority prior to determination.

The approved plan shall be adhered to throughout the construction period.

Reason: To protect the designated Site of Importance for Nature Conservation in line with 2018 Draft Plan GI2.

13 Prior to occupation, a "lighting design strategy for biodiversity" for driveway and front of the houses shall be submitted to and approved in writing by the local planning authority. The strategy shall:

a) identify those areas/features on site that are particularly sensitive for bats and that are likely to cause disturbance along important routes used to access key areas of their territory, for example, for foraging; and

b) show how and where external lighting will be installed (through the provision of appropriate lighting contour plans and technical specifications) so that it can be clearly demonstrated that areas to be lit will not disturb or prevent the above species using their territory or having access to their breeding sites and resting places.

All external lighting shall be installed in accordance with the specifications and locations set out in the approved strategy, and these shall be maintained thereafter in accordance with the strategy. Under no circumstances should any other external lighting be installed without prior consent from the local planning authority.

Reason: To ensure that the introduction of artificial light does not mean that

protected species active at night (bats) are not disturbed or discouraged from using areas of existing habitat.

- 14 LC1 Land contamination - Site investigation
- 15 LC2 Land contamination - remediation scheme
- 16 LC3 Land contamination - remedial works
- 17 LC4 Land contamination - unexpected contam
- 18 HWAY13 Access to be improved

19 Prior to commencement of the development, a Construction Environmental Management Plan (CEMP) for minimising the creation of noise, vibration and dust during the demolition, site preparation and construction phases of the development shall be submitted to and approved in writing by the Local Planning Authority. The CEMP must include a site specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and include a package of mitigation measures commensurate with the risk identified in the assessment. All works on site shall be undertaken in accordance with the approved scheme, unless otherwise agreed in writing by the Local Planning Authority.

NOTE: For noise details on hours of construction, deliveries, types of machinery to be used, use of quieter/silenced machinery, use of acoustic barriers, prefabrication off site etc, should be detailed within the CEMP. Where particularly noisy activities are expected to take place then details should be provided on how they intend to lessen the impact i.e. by limiting especially noisy events to no more than 2 hours in duration. Details of any monitoring may also be required, in certain situation, including the location of positions, recording of results and identification of mitigation measures required.

For vibration details should be provided on any activities which may results in excessive vibration, e.g. piling, and details of monitoring to be carried out. Locations of monitoring positions should also be provided along with details of standards used for determining the acceptability of any vibration undertaken. In the event that excess vibration occurs then details should be provided on how the developer will deal with this, i.e. substitution of driven pile foundations with auger pile foundations. Ideally all monitoring results should be recorded and include what was found and mitigation measures employed (if any).

With respect to dust mitigation, measures may include, but would not be restricted to, on site wheel washing, restrictions on use of unmade roads, agreement on the

routes to be used by construction traffic, restriction of stockpile size (also covering or spraying them to reduce possible dust), targeting sweeping of roads, minimisation of evaporative emissions and prompt clean up of liquid spills, prohibition of intentional on-site fires and avoidance of accidental ones, control of construction equipment emissions and proactive monitoring of dust. Further information on suitable measures can be found in the dust guidance note produced by the Institute of Air Quality Management, see <http://iaqm.co.uk/guidance/>. The CEMP must include a site specific risk assessment of dust impacts in line with the IAQM guidance note and include mitigation commensurate with the scale of the risks identified.

Lighting details should be provided for any artificial lighting to be provided on site, along with details of measures which will be used to minimise impact, such as restrictions in hours of operation, location and angling of lighting.

In addition to the above the CEMP should provide a complaints procedure, so that in the event of any complaint from a member of the public about noise, dust, vibration or lighting the site manager has a clear understanding of how to respond to complaints received. The procedure should detail how a contact number will be advertised to the public, what will happen once a complaint had been received (i.e. investigation), any monitoring to be carried out, how they intend to update the complainant, and what will happen in the event that the complaint is not resolved. Written records of any complaints received and actions taken should be kept and details forwarded to the Local Authority every month during construction works by email to the following addresses: public.protection@york.gov.uk and planning.enforcement@york.gov.uk.

Reason: To protect the amenity of the locality

20 All demolition and construction works and ancillary operations, including deliveries to and dispatch from the site shall be confined to the following hours:

Monday to Friday 0800 to 1800 hours

Saturday 0900 to 1300 hours

Not at all on Sundays and Bank Holidays

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

21 HWAY14 Access to be approved, details reqd

22 The development hereby permitted shall not come into use until the following highway works (which definition shall include works associated with any Traffic Regulation Order required as a result of the development, signing, lighting, drainage

and other related works) have been carried out in accordance with details which shall have been previously submitted to and approved in writing by the Local Planning Authority, or arrangements entered into which ensure the same.

Change of alignment of Cherry Lane

Reason: In the interests of the safe and free passage of highway users.

23 A three stage road safety audit carried out in line with advice set out in GG119 Road safety audit (formerly HD 19/15), and guidance issued by the council, will be required for the proposed new access to the development. Reports for Stages 1 and 2 must be submitted to and agreed in writing by the LPA prior to works commencing on site. The Stage 3 report must be submitted to and agreed in writing by the LPA prior to occupation. The development shall be carried out in accordance with the approved details.

Reason: To minimise the road safety risks associated with the changes imposed by the development.

24 A detailed method of works statement identifying the programming and management of site clearance/excavation/preparatory and construction works shall be submitted to and approved in writing by the Local Planning Authority prior to development commencing. The statement shall include at least the following information:

Details of access/ egress and turning within the site for contractors and other users of the area to manage conflict between vehicles accessing the site and users of the lane (including secondary school children).

The development shall be carried out in accordance with the approved statement.

Reason: To ensure that the development can be carried out in a manner that will not be to the detriment of amenity of local residents, free flow of traffic or safety of highway users. Details are required prior to the commencement of development to ensure demolition/ construction traffic does not impact on the amenity of local residents.

25 HWAY40 Dilapidation survey

26 The dwellings hereby approved shall achieve the following measures:

- At least a 19% reduction in Dwelling Emission Rate compared to the target fabric

energy efficiency rates as required under Part L1A of the Building Regulations 2013).

- A water consumption rate of 110 litres per person per day (calculated as per Part G of the Building Regulations).

- A reduction in carbon emissions of at least 28% compared to the target emission rate as required under Part L of the Building Regulations.

Prior to first occupation of each dwelling details of the measures undertaken to secure compliance with this condition shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: In the interests of sustainable design and in accordance with policies CC1 and CC2 of the Publication Draft Local Plan 2018.

7.0 INFORMATIVES:

Notes to Applicant

1. STATEMENT OF THE COUNCIL`S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome:

Requested changes to highway access

Required S106 contributions in accordance with Local Plan policy.

2. Nesting Birds

The applicant is reminded that, under the Wildlife and Countryside Act 1981, as amended (section 1), it is an offence to remove, damage or destroy the nest of any wild bird while that nest is in use or being built. Planning consent for a development does not provide a defence against prosecution under this act.

Trees and scrub are likely to contain nesting birds between 1st March and 31st August inclusive. Trees and scrub are present on the application site and are to be assumed to contain nesting birds between the above dates, unless a recent survey has been undertaken by a competent ecologist to assess the nesting bird activity on site during this period and has shown it is absolutely certain that nesting birds are

not present.

3. Electric Vehicle Charging Point advice

Any future Electric Vehicle Charging Points need to be professionally installed. The installation process routinely involves wall mounting a charge point on an exterior wall or garage and connecting it safely to the mains electricity supply. All electrical circuits/installations shall comply with the electrical requirements in force at the time of installation.

In the UK, there is a government-grant scheme available to help reduce the cost of installing a home EV charge point. For more information on the scheme see the OLEV website <https://www.gov.uk/government/collections/governmentgrants-for-low-emission-vehicles>

The above requirement does not preclude the installation of Electric Vehicle Charge Point from the outset, if desired. Details of passive provision to be included within household pack for first occupant, to include location of proposed Electric Vehicle Recharge Point, trunking/ducting provided and details of distribution board location and capacity.

4. INFORMATIVE:

You are advised that prior to starting on site consent will be required from the Highway Authority for the works being proposed, under the Highways Act 1980 (unless alternatively specified under the legislation or Regulations listed below). For further information please contact the officer named:

Works in the highway - Section 171 - Vehicle Crossing - Section 184 - (01904) 551550 - streetworks@york.gov.uk

Pavement Cafe Licence - Section 115 - Annemarie Howarth (01904) 551550 - highway.regulation@york.gov.uk

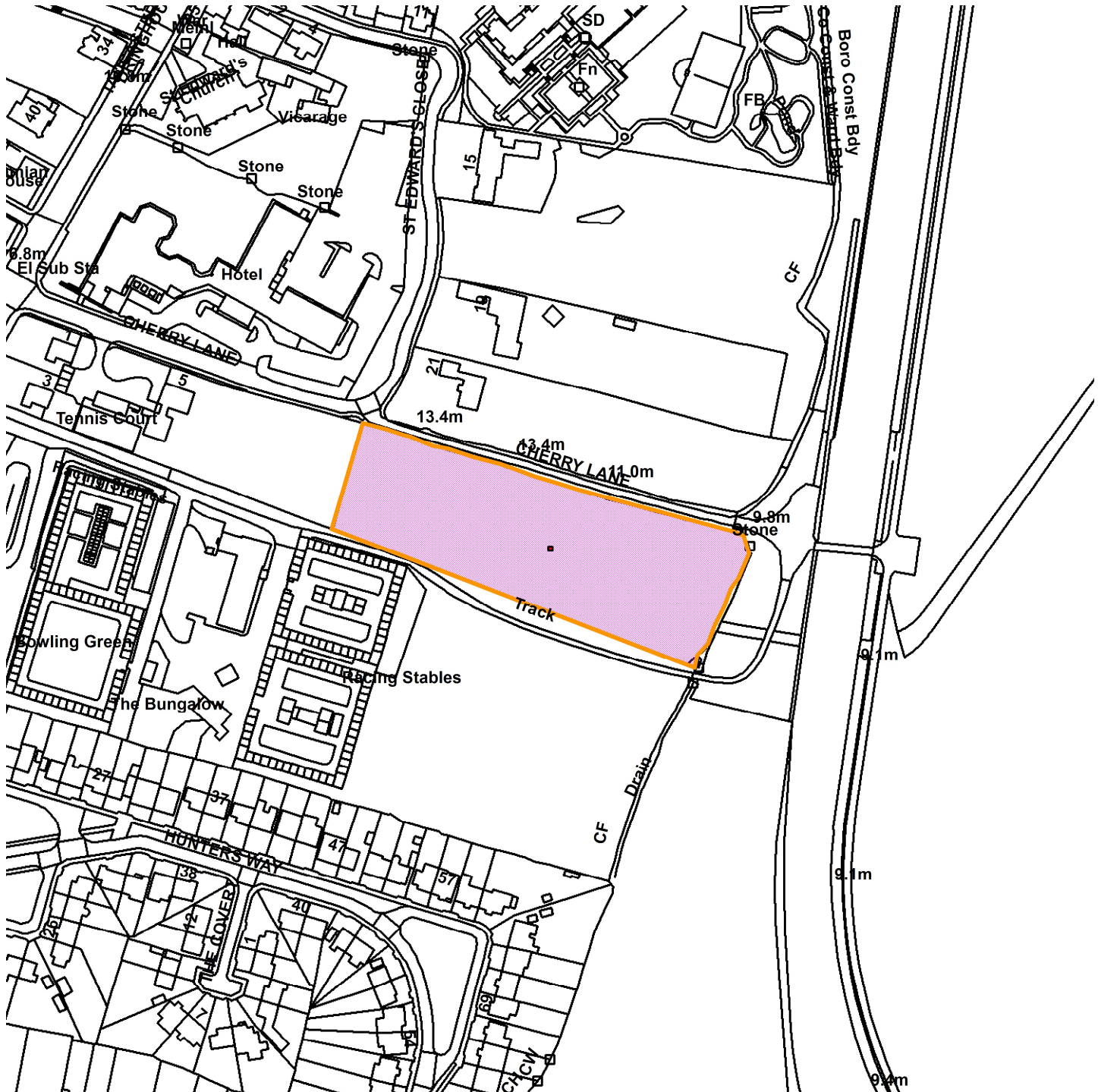
Contact details:

Case Officer: Alison Stockdale

Tel No: 01904 555730

Site To The Rear Of 5 Cherry Lane, York

19/02729/OUT



Scale : 1:2454

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Organisation	City of York Council
Department	Economy & Place
Comments	Site Location Plan
Date	11 August 2020
SLA Number	

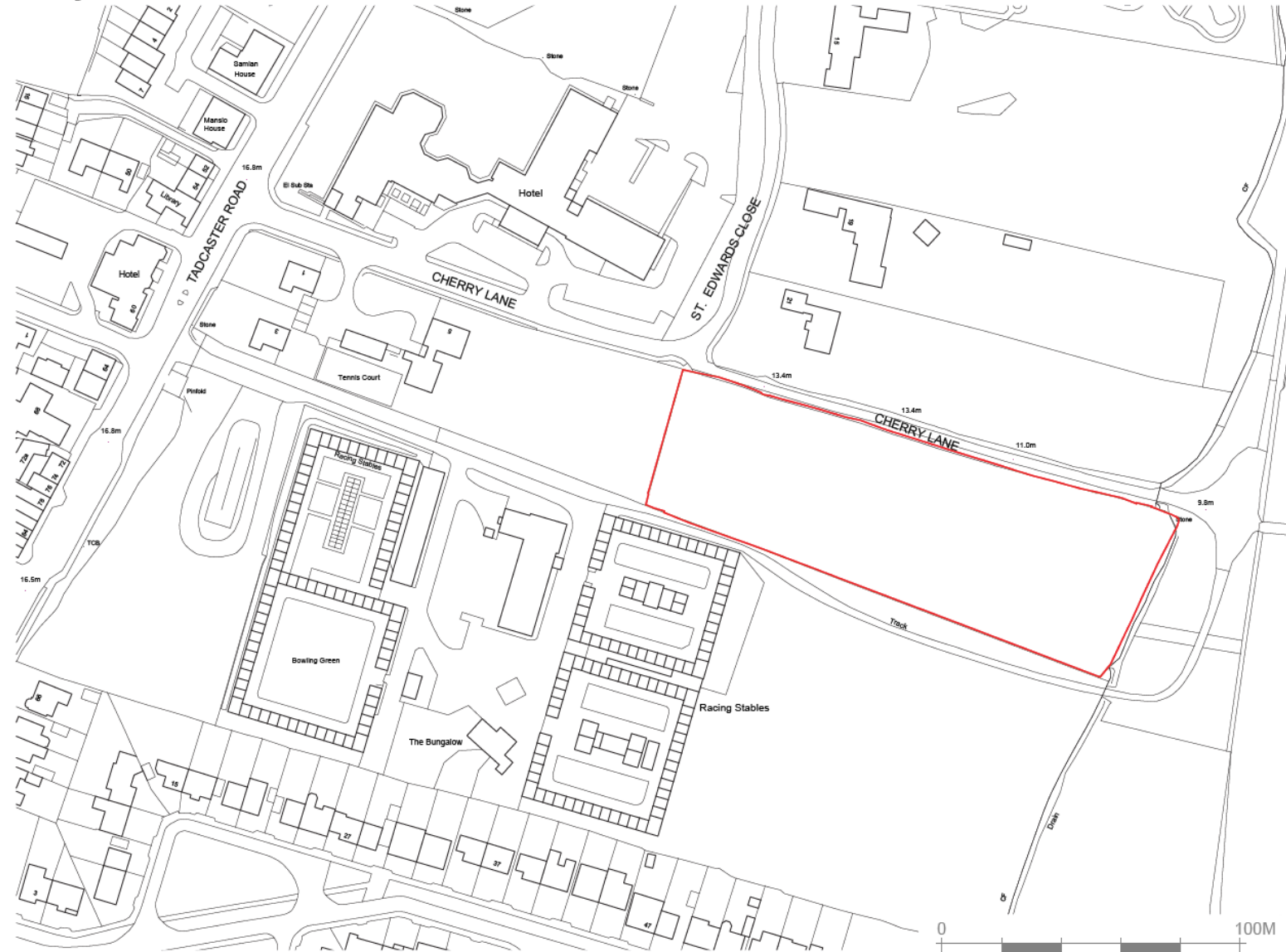
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19/02729/OUT - Site To The Rear Of 5 Cherry Lane, York

Outline application for the erection of 5no. detached dwellings with means of access

Site Location Plan

Proposed Residential Development at Cherry Lane, York



Rev	By	Note	Date



53 The Tansley, Lawrence Street, York, YO10 3WH T: 01904 633772
F: 01904 633779 E: mail@pra-architects.com W: www.pra-architects.com

PROJECT: Cherry Lane, York

TITLE: Red Line Site Location Plan

CLIENT: [Blank]

DATE: 01.10.19 SCALE: 1:1250@A3

DWG. NO: Y81:948.01 REVISION: -

DRAWN: PB CHECKED: [Blank]

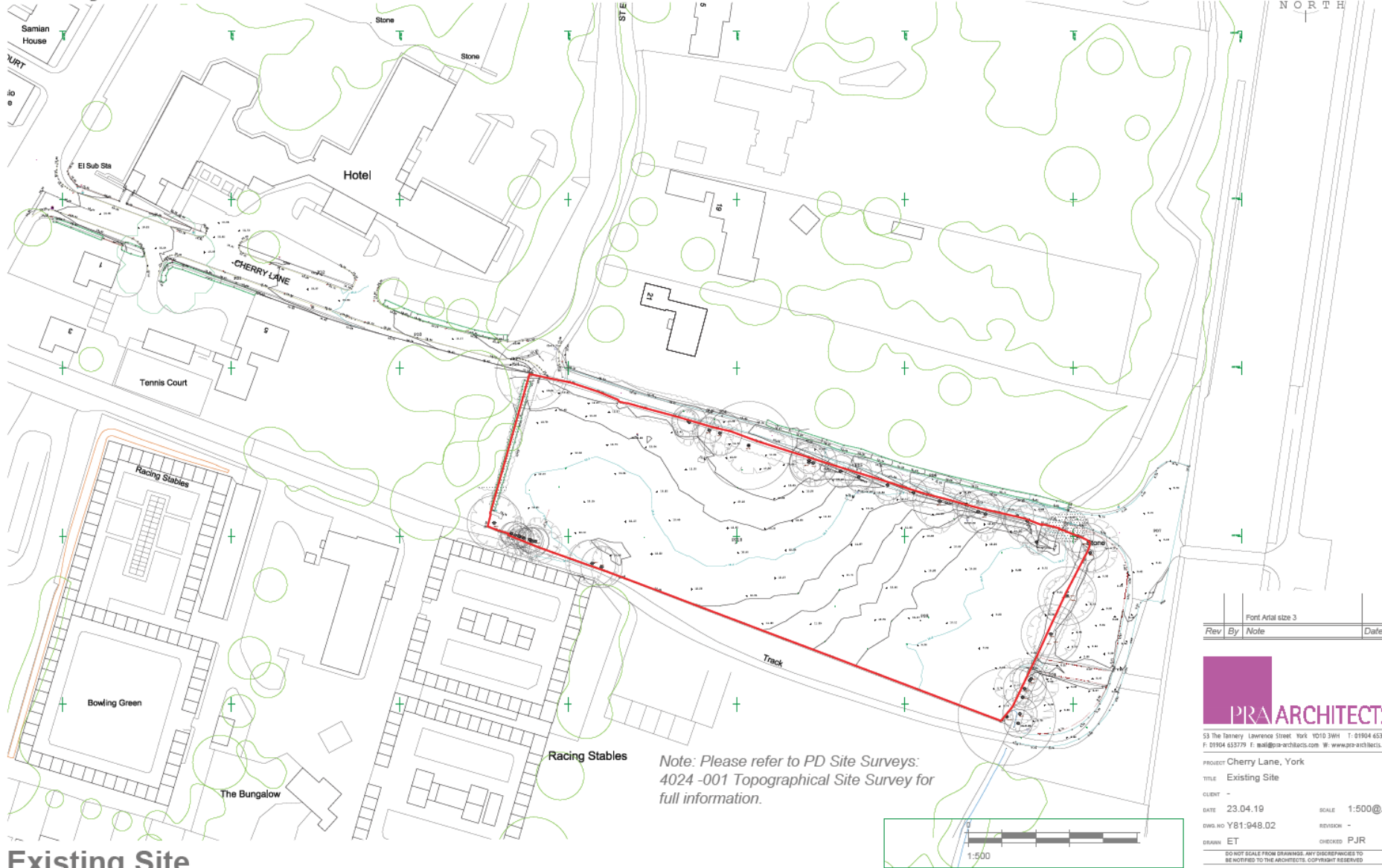
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Red Line Site Location Plan

Existing Site Plan

Proposed Residential Development at Cherry Lane, York



Illustrative Site Layout

Proposed Residential Development at Cherry Lane, York



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Illustrative Site Layout

Rev	By	Note	Date

PRA ARCHITECTS
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PROJECT: Cherry Lane, York
 TITLE: Illustrative Site Layout
 CLIENT: -
 DATE: 23.04.19 SCALE: 1:500@A1
 DRAW NO: Y81-948.03 CHECKED: PJR
 DRAWN: ET

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Illustrative Site Layout in Context



Illustrative Site Layout with Local Context

City of York Council Area Planning Sub Committee Meeting - 20th August 2020



Illustrative Streetscape

Proposed Residential Development at Cherry Lane, York



View from within the site



View from Cherry Lane



Rev	By	Note	Date

PRA ARCHITECTS
 53 The Barnyard, Lawrence Street, York, YO1 8 3WH T: 01904 653772
 F: 01904 653770 E: mail@pra-architects.com W: www.pra-architects.com

PROJECT: Cherry Lane, York
 TITLE: Illustrative Streetscape
 CLIENT:
 DATE: 12.03.19 SCALE: 1:250@A1
 DRAWING NO: YB1-948.05 REVISION:
 DRAWN: ET CHECKED: PJR
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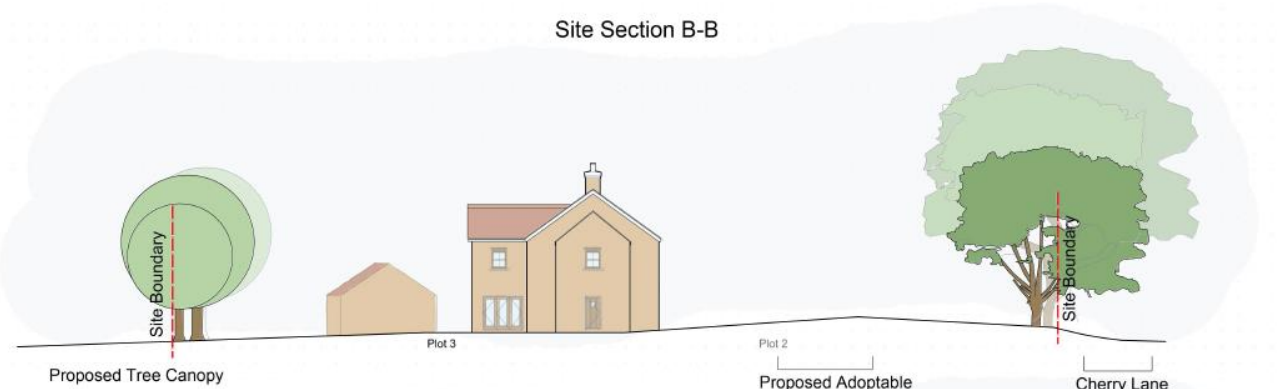


Illustrative Streetscape

City of York Council Area Planning Sub Committee Meeting - 20th August 2020

Illustrative Site Sections

Proposed Residential Development at Cherry Lane, York



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Illustrative Site Sections

City of York Council Area Planning Sub Committee Meeting - 20th August 2020



Rev	By	Note	Date

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PRA ARCHITECTS
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PROJECT: Cherry Lane, York
 TITLE: Illustrative Site Sections
 CLIENT: Shepherd Homes
 DATE: 23.04.19 SCALE: 1:200@A2
 DRAWN BY: Y81-948.06 REVISION: -
 CHECKED BY: ET REVISION: PJR

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COMMITTEE REPORT

Date: 20 August 2020 **Ward:** Osbaldwick and Derwent
Team: East Area **Parish:** Osbaldwick Parish
Council

Reference: 19/02738/FULM
Application at: Etas Ltd Bacchus House Link Road Court Osbaldwick Link Road
Osbaldwick
For: Proposed change of use from offices (Use Class B1) to health
care unit (Use Class D1), and single storey extension and flat
roof canopy to the north west elevation.
By: Mr Chris Parker
Application Type: Major Full Application
Target Date: 26 August 2020
Recommendation: Approve subject to Section 106 Agreement

1.0 PROPOSAL

1.1 Planning permission is sought for the change of use of the building from Office (B1 Use Class) to a community team base for York Children & Adolescent Mental Health Service (CAMHS) (Use Class D1). In addition, a single storey extension to the front/north elevation is proposed, together with a canopy. The other alterations include: creating window openings; creating door openings in the north and west elevations. The total proposed floor areas would be 1047sqm.

1.2 The building is part of a complex of office buildings built in 2000. The site is within Flood Zone 1. The eastern side of Osbaldwick Link Road is commercial in character. There is residential development to the west of the link road.

1.3 The unit is situated on a business park accessed via Osbaldwick Link Road, and currently provides 1,035 sqm of B1 office space. Officers understand this is not the creation of additional jobs rather the relocation of a unit currently located at Lime Trees on Shipton Road. The applicant has advised that due to increased investment the service has expanded and there is insufficient space at Lime Trees to accommodate the team and the increased therapy they are able to deliver. Lime Trees would be vacated once the proposed scheme has been undertaken.

1.4 Proposed opening hours of the unit would be 08.00 to 18.00 hours with out of hours being based at the Foss Park Hospital. There are 10 proposed interview/group rooms. It is expected that each interview/therapy room would be used for a maximum of 6 sessions per day, lasting approx. 1.5 hours with follow ups of approx. 45mins. The expectation is that each room would be in use for average 5 hours 20 mins per day. Group sessions will be run approx. twice a week, with a maximum of 10 patients.

1.5 During the application process additional information regarding highway safety and parking has been submitted. In addition, information has been submitted regarding the marketing of the unit, as well as how the unit would be used. Information on BREEAM has also been submitted.

2.0 POLICY CONTEXT

2.1 The City of York Draft Local Plan Incorporating the Fourth Set of Changes was approved for Development Management purposes in April 2005:

GP1 Design

GP4a Sustainability

E3b Existing and Proposed Employment Sites

C1 Community Facilities

2.2 The Publication Draft York Local Plan (2018)

EC2 Loss of Employment Land

HW5 Healthcare Services

D1 Placemaking

D11 Extensions and Alterations to Existing Buildings

ENV1 Air Quality

ENV2 Managing Environmental Quality

ENV5 Sustainable Drainage

T1 Sustainable Access

CC2 Sustainable design and Construction of New Development

2.3 Please see the Appraisal Section (5.0) for national and local policy context.

3.0 CONSULTATIONS

INTERNAL CONSULTATIONS

HIGHWAY NETWORK MANAGEMENT

3.1 No objections subject to the following conditions: a footway extension to connect Link Road Court to a tactile crossing, intersecting Osbaldwick Link Road; Travel Plan, submission of cycle parking details; safety audit. Also a legal agreement for potential restrictions on nearby roads. HNM have been in contact with the agents and it is understood that that the above has been agreed.

3.2 A Transport Assessment has been submitted in support of the application. This has been amended (by way of addendums) to reflect a robust assessment based on staff surveys and rotas. The anticipated traffic generation is not deemed to have a significant impact on the highway from that of its existing use.

3.4 The Transport Assessment details the expected level of parking created as a result of the proposed use. It was identified that the existing office (Shipton Road) was underprovided for in terms of parking provision for this area. The headline figures from the proposed assessment reveal that overspill parking is to be expected at certain times of the day, albeit to a lesser extent than the previous use of the building (Maximum 18 spaces from the previous 28 spaces). Due to the impact being a reduction of 10 on-street car parking spaces from that of which has been previously approved, HNM do not consider that it can object. HNM have negotiated measures with the applicant to mitigate the impact of parking and encourage sustainable transport modes.

3.5 To encourage travel by public transport, the applicant has agreed to provide a footway extension to connect Link Road Court to a tactile crossing, intersecting Osbaldwick Link Road. Given that the development expects overspill parking, there is some concern that that the use of the site would result indiscriminate parking and obstruct the free flow of traffic on streets in the vicinity of the site or obscure the visibility of the newly proposed pedestrian crossing. As such, it is considered that it would be reasonable to secure a contribution via a legal agreement towards parking restrictions (to include assessment, processing and implementing Traffic Regulation Orders) on the highway within 500m of the development. These would be processed if deemed necessary within the first 5 years of occupation, to protect the proposed crossing and junctions as a result of the projected parking generation. The trust has indicated that they would be prepared to fund the £5K required.

LEAD LOCAL FLOOD AUTHORITY

3.6 No objections

PUBLIC PROTECTION

3.7 No objections, subject to following conditions: 1 no. electric vehicle recharging point; details of all machinery audible outside of the premises; restriction of the hours of deliveries and waste removal; and adequate facilities for the treatment and extraction of cooking odours. Request Development Informative.

FORWARD PLANNING

3.8 Given the advanced stage of the emerging Local Plan's preparation, the lack of significant objection to the emerging policies relevant to this application and the stated consistency with the NPPF, Forward Planning would advise that the policy requirements of emerging Local Plan policies EC2, HW5, T1, CC2, ENV1, ENV4 and ENV5 should be applied with moderate weight.

3.9 The key policy test for the change of use from B1 to D1 is whether the site is needed for employment use; the applicant should submit a statement to satisfy the

policy requirements of EC2. The views of the Council's Economic Development Team should also be sought.

3.10 If minded to accept the loss of the site's employment use, Forward Planning would not wish to raise a policy objection to the principle of the proposed use, which accords with policy HW5. Forward Planning advise that the following further policy requirements are addressed:

- o Re policy CC2, non-residential change of use should achieve BREEAM 'excellent';
- o Re policy ENV1, submit an Emissions Statement, identifying all new emissions likely to arise as a result of the proposal and demonstrating how these will be minimised and mitigated against as part of the development;
- o Agreement re extent of requirements under ENV4 and ENV5.

ECONOMIC DEVELOPMENT UNIT

3.11 No comments received.

EXTERNAL CONSULTATIONS/REPRESENTATIONS

OSBALDWICK PARISH COUNCIL

3.12 No comments received.

4.0 REPRESENTATIONS

4.1 No representations have been received to date.

5.0 APPRAISAL

PLANNING POLICY

5.1 Section 38(6) of the Planning and Compensation Act 2004 requires that determinations be made in accordance with the development plan unless material considerations indicate otherwise.

PUBLICATION DRAFT YORK LOCAL PLAN (2018)

5.2 The Publication Draft City of York Local Plan 2018 ('2018 Draft Plan') was submitted for examination on 25 May 2018. Phase 1 of the hearings into the examination of the Local Plan took place in December 2019. In accordance with paragraph 48 of the NPPF as revised in February 2019, the relevant 2018 Draft Plan policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012. (NB: Under transitional arrangements plans submitted for examination before 24 January 2019 will be assessed against the 2012 NPPF).

DEVELOPMENT CONTROL LOCAL PLAN (2005)

5.3 The Development Control Local Plan Incorporating the Fourth Set of Changes was approved for development management purposes in April 2005 (DCLP). Whilst the DCLP does not form part of the statutory development plan, its policies are considered to be capable of being material considerations in the determination of planning applications. Where policies relevant to the application are consistent with those in the NPPF (as revised in February 2019), the weight that can be afforded to them is very limited.

NATIONAL PLANNING POLICY FRAMEWORK (NPPF)

5.4 The revised National Planning Policy Framework was published February 2019 (NPPF) and its planning policies are a material consideration in the determination of planning applications. The NPPF sets out the presumption in favour of sustainable development in paragraph 11.

LOSS OF OFFICE SPACE

5.6 Policy DP1 of the Publication Draft Local Plan set out the approach taken in the plan is that development will aim to ensure York fulfil its role as a key economic driver. Policy EC2 considers the loss of employment land. Policy E3b of the 2005 DCLP states site in employment use shall be retained in their current use class unless there is a sufficient supply of employment land to meet both immediate and longer term requirements in both qualitative and quantitative terms or the development for other appropriate uses will lead to significant benefits to the local economy.

5.7 Policy EC2 'Loss of Employment Land' expects proposals which involve the loss of land and/or buildings which are either identified, currently used or were last used for employment uses, to be accompanied by a statement to the satisfaction of the Council demonstrating that the site is not viable and that the proposal would not lead to the loss of an employment site that is necessary to meet employment needs during the plan period.

5.8 Policy HW5 'Healthcare Services' of the Draft Local Plan (2018) supports the provision of accessible new or enhanced primary and secondary care services to both improve existing provision and support planned growth. This is reflected in NPPF's definition of sustainable development, encouraging strong, vibrant and healthy communities, making sufficient provision for facilities such as health infrastructure, and, at para 121, to "make more effective use of sites that provide community services such as schools and hospitals, provided this maintains or improves the quality of service provision".

5.9 The NPPF is clear in that Planning should operate to encourage and not act as an impediment to sustainable growth. Some marketing information has been submitted however it is not considered to demonstrate that the building is no longer required as a B1 use. From the submitted information it appears that once the building was to be marketed the estate agents approached the applicant. As such, 6 months of adequate marketing has not been demonstrated. The proposed change of use would result in the loss of existing office space, however it would be replaced by a medical use that would provide a valuable facility in the city. The NPPF makes it clear that there is a social objective to sustainability, including supporting health.

HIGHWAY IMPLICATIONS

5.10 The NPPF encourages development that is sustainably located and accessible. Paragraph 108 requires that all development achieves safe and suitable access for all users. It advises at paragraph 109 that development should only be prevented or refused on highway grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe. Further, paragraph 110 requires development to, inter alia, give priority first to pedestrians and cycle movements and create places that are safe, secure and attractive thereby minimising the scope for conflicts between pedestrians, cyclists and vehicles. Policy T1 of the 2018 Draft Local Plan supports the approach of the NPPF in that it seeks the safe and appropriate access to the adjacent adopted highway, giving priority to pedestrians and cyclists.

5.11 The Transport Assessment details the expected level of parking created as a result of the proposed use. It was identified that the existing office (Shipton Road) was underprovided for in terms of parking provision for this area. The assessment reveals that overspill parking is to be expected at certain times of the day at the application site, albeit to a lesser extent than the previous office use of the building (Maximum 18 spaces from the previous 28 spaces). Highway Network Management (HNM) do not object to the 10 on-street parking spaces. HNM have negotiated measures with the applicant to mitigate the impact of parking and encourage sustainable transport modes. HNM have advised that the applicant has agreed to providing a footway extension to connect Link Road Court to a tactile crossing,

intersecting Osbaldwick Link Road, this can be sought via condition. Given that the development expects overspill parking, there is some concern that the use of the site would result in indiscriminate parking and obstruct the free flow of traffic on streets in the vicinity of the site or obscure the visibility of the newly proposed pedestrian crossing. As such, it is considered that it would be necessary to secure a contribution via a legal agreement towards parking restrictions (to include assessment, processing and implementing Traffic Regulation Orders) on the highway within 500m of the development. These would be processed if deemed necessary within the first 5 years of occupation, to protect the proposed crossing and junctions as a result of the projected parking generation.

5.12 To complement the aims and objectives of the NHS trust, and reduce the impact of parking on the highway as a result of projected overspill parking, a Travel Plan is conditioned to take advantage of sustainable travel options obtainable to the 53 staff expected, thus reduce reliance on the private car in accordance with the sentiments of NPPF 2018.

5.13 Any facilities should be easily accessible by foot, bike and public transport in line with policy T1 'Sustainable Access'. Osbaldwick Link Road is served by a frequent bus route (number 6), offering connections to the city centre during and beyond the facility's proposed opening hours. Provision of vehicle re-charging points can be sought via condition.

VISUAL AMENITY AND CHARACTER

5.14 Chapter 12 of the NPPF gives advice on design, placing great importance to the design of the built environment. At paragraph 130, it advises against poor quality design that fails to take the opportunities available for improving the character and quality of an area and the way it functions. The advice in Chapter 12 is reflected in DCLP policy GP1 (Design) and 2018 Draft Local Plan policy D1 (Placemaking) and, therefore, these policies are considered to accord with the NPPF. The policies require new development to respect or enhance its local environment and be of an appropriate density, layout, scale, mass and design compatible with neighbouring buildings and using appropriate materials. The alterations to the building and the proposed extension are considered to be relatively minor and have a neutral impact on the appearance of the building and the character of the surrounding commercial area.

SUSTAINABILITY

5.15 Policy CC2 'Sustainable Design and Construction of New Development' states that developments which demonstrate high standards of sustainable design and construction will be encouraged. Development proposals will be required to demonstrate energy and carbon dioxide savings in accordance with the energy hierarchy and water efficiency. The policy requires that non-residential change of

use will be required to achieve BREEAM 'Excellent'. The Local Plan policy CC2 "Sustainable Design and Construction of New Development" is considered to have moderate weight given the evidence base that supports it.

5.16 The agent has advised that the development is unable to achieve BREEAM 'Excellent'. The agent advises that the works to convert the building from an office to a D1 use would be minimal and if they were required to upgrade to the building to BREEAM 'Excellent' there would be a question of financial viability. As such the proposed development would not comply with Policy C2. There is a compelling requirement for the retention of this facility within York, together with the enhanced capacity that the use of this building would allow. The Local Plan policy is considered to have moderate weight given the evidence base that supports it. The NPPF requires the presumption in favour of sustainable development should apply. This means that the application should be approved unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the NPPF taken as a whole. On balance it is considered that the non-compliance with Policy CC2, does not significantly and demonstrably outweigh the public benefits of the proposed development.

6.0 CONCLUSION

6.1 The proposed development would result in the loss of office use/accommodation, however the proposed use is considered to employ similar numbers to the permitted use. The proposed use would provide an expansion to the capacity of this service and would benefit the health of the wider population for the city and the surrounding area. Officers recommend approval of the scheme subject to the completion and signing of a Section 106 agreement covering introduction of parking restrictions to the surrounding area.

6.2 The proposal falls within class D1 (Non-residential institutions) of the Use Classes Order 1987 (as amended). Permitted uses within D1 use class include clinics, health centres, crèches, day nurseries, day centres, schools, art galleries (other than for sale or hire), museums, libraries, halls, places of worship, church halls, law court, non-residential education and training centres. Change of use to a different use within the same use class does not normally require planning permission. Whilst the currently proposed use is in principle acceptable in this area - subject to appropriate conditions - the characteristics of other uses in class D1 may make those uses unacceptable. A condition should therefore be attached limiting the planning permission to the proposed clinic use only and no other use within class D1.

7.0 RECOMMENDATION: Approve subject to Section 106 Agreement

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans:-

Drawing Number 2047.005 'Location Plan' received 20 December 2019;
Drawing Number 2047.200 'Proposed_Site_Plan' received 20 December 2019;
Drawing Number 2047.021 revision B 'Proposed_Floor_Plans' received 20 December 2019;
Drawing Number 2047.022 Revision A 'Proposed_Elevations_&_Sections' received 10 March 2020;

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 VISQ1 Matching materials

4 The premises shall be used as a mental health care unit and for no other purpose, including any other purpose in Class D1 in the Schedule of the Town and Country Planning (Use Classes) Order 1987 or in any provision equivalent to that Class in any Statutory Instrument revoking and re-enacting that Order.

Reason: Whilst the proposed use is considered acceptable in this area, the characteristics of other uses within class D1 may make such uses unacceptable in terms of highway and parking impacts and the impact on the surrounding road network. The characteristics of other uses within class D1 may make such uses incompatible with the surrounding uses and character of the area. This condition is therefore required to enable the Local Planning Authority to re-assess alternative uses which, without this condition, may have been carried on without planning permission by virtue of Article 3 of the Town and Country Planning (Use Classes) Order 1987.

5 Prior to the first occupation of the development 1 Electric Vehicle Recharging Point(s) the specification shall be submitted to and approved in writing by the Local Planning Authority. In addition, a minimum of 1 additional parking bays should be identified for the future installation of additional Electric Vehicle Charging Points. Such additional bays should be provided with all necessary ducting, cabling and groundwork to facilitate the addition of Electric Vehicle Charge Points in the future, if required (passive provision). The locations of these additional bays shall be submitted to and agreed in writing by the Local Planning Authority. Charging points should be located in a prominent position on the site and should be for the exclusive use of zero emission vehicles. Within 3 months of the first occupation of the development, an Electric Vehicle Recharging Point Management Plan shall be submitted in writing and approved by the Local Planning Authority. The plan shall detail the management, maintenance, servicing and access arrangements for each Electric Vehicle Recharging Point for a period of 10 years The approved plan shall be adhered to.

INFORMATIVE:

Electric Vehicle Charging Points should incorporate a suitably rated 32A 'IEC 62196' electrical socket to allow 'Mode 3' charging of an electric vehicle. The exact specification is subject to agreement in writing with the council.

Charging points should be located in a prominent position on the site and should be for the exclusive use of zero emission vehicles. Parking bay marking and signage should reflect this.

All electrical circuits/installations shall comply with the electrical requirements in force at the time of installation

Reason: To promote and facilitate the uptake of electric vehicles on the site in line with the Council's Low Emission Strategy (LES) and the National Planning Policy Framework (NPPF).

6 Details of all machinery, plant and equipment to be installed in or located on the premises, which is audible outside of the premises, shall be submitted to the local planning authority for written approval. These details shall include average sound levels (LAeq), octave band noise levels and any proposed noise mitigation measures. The machinery, plant or equipment and any approved noise mitigation measures shall be fully implemented and operational before the development comes into use and shall be maintained thereafter.

Note: The combined rating level of any building service noise associated with plant or equipment at the site should not exceed the representative LA90 1 hour during the hours of 07:00 to 23:00 or representative LA90 15 minutes during the hours of 23:00 to 07:00 at 1 metre from the nearest noise sensitive facades when assessed in accordance with BS4142: 2014, inclusive of any acoustic feature corrections associated with tonal, impulsive, distinctive or intermittent characteristics.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

7 Upon completion of the development, delivery vehicles and waste removal vehicles to the development shall be confined to the following hours:

Monday to Friday 08:00 to 18:00 hours

Saturday 09:00 to 13:00 hours and not at all on Sundays and Bank Holidays

Reason: To protect the amenity of occupants of the nearby properties from noise.

8 There shall be adequate facilities for the treatment and extraction of cooking

odours. Details of the extraction plant or machinery and any filtration system required shall be submitted to the local planning authority for written approval. Once approved it shall be installed and fully operational before the development comes into use and shall be maintained and serviced thereafter in accordance with manufacturer guidelines.

Note: It is recommended that the applicant refers to the updated Guidance produced by EMAQ in September 2018 titled "Control of Odour and Noise from Commercial Kitchen Exhaust Systems (September 2018)" for further advice on how to comply with this condition. The applicant shall provide information on the location and level of the proposed extraction discharge, the proximity of receptors, size of kitchen or number of covers, and the types of food proposed. A risk assessment in accordance with APPENDIX 3 of the EMAQ guidance shall then be undertaken to determine the level of odour control required. Details should then be provided on the location and size/capacity of any proposed methods of odour control, such as filters, electrostatic precipitation, carbon filters, ultraviolet light/ozone treatment, or odour neutraliser, and include details on the predicted air flow rates in m³/s throughout the extraction system.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

9 HWAY18 Cycle parking details to be agreed

10 HWAY19 Car and cycle parking laid out

11 The development hereby permitted shall not come into use until the following highway works: Footway extension to the junction of Osbaldwick Link Road Court and tactile crossing across Osbaldwick Link Road, amendments to the cycle/footpath opposite as indicatively shown on drawing YK6481-1PD-001 dated 24.06.20 included within the submitted addendum to the Transport Assessment dated 24/06/20 (includes works associated with any Traffic Regulation Order required as a result of the development, signing, lighting, drainage and other related works) have been carried out in accordance with details which shall have been previously submitted to and approved in writing by the Local Planning Authority, or arrangements entered into which ensure the same.

Informative: It is anticipated that the works will be carried under a Section 278 Highway agreement or similar.

Reason: In the interests of the safe and free passage of highway users.

12 A three stage road safety audit carried out in line with advice set out in GG119 Road safety audit (formerly HD 19/15), and guidance issued by the council, will be required for the proposed pedestrian crossing on Osbaldwick Link Road. Reports for

Stages 1 and 2 shall be submitted to and agreed in writing by the Local Planning Authority prior to works commencing on site. The Stage 3 report shall be submitted to and agreed in writing by the Local planning Authority prior to occupation. The development shall be carried out in accordance with the agreed reports.

Reason: To minimise the road safety risks associated with the changes imposed by the development.

13 No part of the development shall be occupied until a Full Travel Plan has been submitted and approved in writing by the LPA. The Travel Plan should be developed and implemented in line with local and national guidelines. The site shall thereafter be occupied in accordance with the aims, measures and outcomes of approved Travel Plan.

Within 12 months of occupation of the site a first year travel survey shall have been submitted to and approved in writing by the LPA. Results of yearly travel surveys shall then be submitted annually to the authority's travel plan officer for approval.

Reason: To promote the usage of sustainable means of transport and reducing the reliance on the private car.

8.0 INFORMATIVES: Notes to Applicant

1. STATEMENT OF THE COUNCIL`S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome:

- Request revised plans
- Request additional information
- Use of conditions
- Use of legal agreement

2. Development Informative

The developer's attention should also be drawn to the following which should be attached to any planning approval as an informative.

(i) All demolition and construction works and ancillary operations, including deliveries to and dispatch from the site shall be confined to the following hours:

Monday to Friday 08.00 to 18.00

Saturday 09.00 to 13.00

Not at all on Sundays and Bank Holidays.

(ii) The work shall be carried out in such a manner so as to comply with the general recommendations of British Standards BS 5228-1:2009 + A1:2014 and BS 5228-2:2009 + A1:2014, a code of practice for "Noise and Vibration Control on Construction and Open Sites".

(iii) Best practicable means shall be employed at all times in order to minimise noise, vibration, dust, odour and light emissions. Some basic information on control noise from construction site can be found using the following link.

https://www.york.gov.uk/downloads/download/304/developers_guide_for_controlling_pollution_and_noise_from_construction_sites

(iv) All plant and machinery to be operated, sited and maintained in order to minimise disturbance. All items of machinery powered by internal combustion engines must be properly silenced and/or fitted with effective and well-maintained mufflers in accordance with manufacturers instructions.

(v) There shall be no bonfires on the site.

(vi) In the event that contamination is found at any time when carrying out the approved development, the findings must be reported in writing immediately to the Local Planning Authority. In such cases, an investigation and risk assessment must be undertaken and where remediation (clean-up) is necessary a remediation scheme must be prepared, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority. Should City of York Council become aware at a later date of suspect contaminated materials which have not been reported as described above, the council may consider taking action under Part 2A of the Environmental Protection Act 1990.

3. AVOIDING DAMAGE TO THE HIGHWAY GRASS VERGE

Applicants/Developers are reminded that great care should be taken to ensure that no damage to the surface or structure of the public highway is caused, by activities relating directly to the approved development (e.g. delivery of building materials via HGV's). The Council is particularly concerned at the increasing impacts and damage occurring to grass verges. This is detrimental to residential amenity, can present safety issues and places an unreasonable financial burden on the Council, if repairs are subsequently deemed necessary. Therefore, applicants/developers are strongly advised to work proactively with their appointed contractors and delivery companies to ensure that their vehicles avoid both parking and manoeuvring on areas of the

public highway (grass verges) which are susceptible to damage. The council wishes to remind applicants that legislation (Highways Act 1980) is available to the authority to recover any costs (incurred in making good damage) from persons who can be shown to have damaged the highway, including verges. If the development is likely to require the temporary storage of building materials on the highway, then it is necessary to apply for a licence to do so. In the first instance please email highway.regulation@york.gov.uk, with details of the site location, planning application reference, anticipated materials, timelines and volume. Please refer to the Council website for further details, associated fees and the application form.

4. DISPOSAL OF COMMERCIAL WASTE

Section 34 of the Environmental Protection Act 1990 places a duty of care on all producers of controlled waste, i.e. businesses that produce, store and dispose of rubbish. As part of this duty, waste must be kept under proper control and prevented from escaping. Collection must be arranged through a registered waste carrier. It is unlawful to disposal of commercial waste via the domestic waste collection service.

Adequate arrangements are required for proper management and storage between collections.

Section 47 of the Environmental Protection Act 1990

The storage of commercial waste must not cause a nuisance or be detrimental to the local area. Adequate storage and collections must be in place. Where the City of York Council Waste Authority considers that storage and/or disposal are not reasonable, formal notices can be served (Section 47 of the Environmental Protection Act 1990). Storage containers cannot be stored on the highway without prior consent of the Highway Authority of City of York Council.

5. INFORMATIVE:

You are advised that this proposal may have an affect on Statutory Undertakers equipment. You must contact all the utilities to ascertain the location of the equipment and any requirements they might have prior to works commencing.

6. Consent for highway works

You are advised that prior to starting on site, consent will be required from the Highways Authority for the works being proposed under the Highways Act 1980 (or legislation/ regulations listed below). For further information, please contact the following:

- Execution of works within existing highway (Section 278) -

development.adoption@york.gov.uk

Contact details:

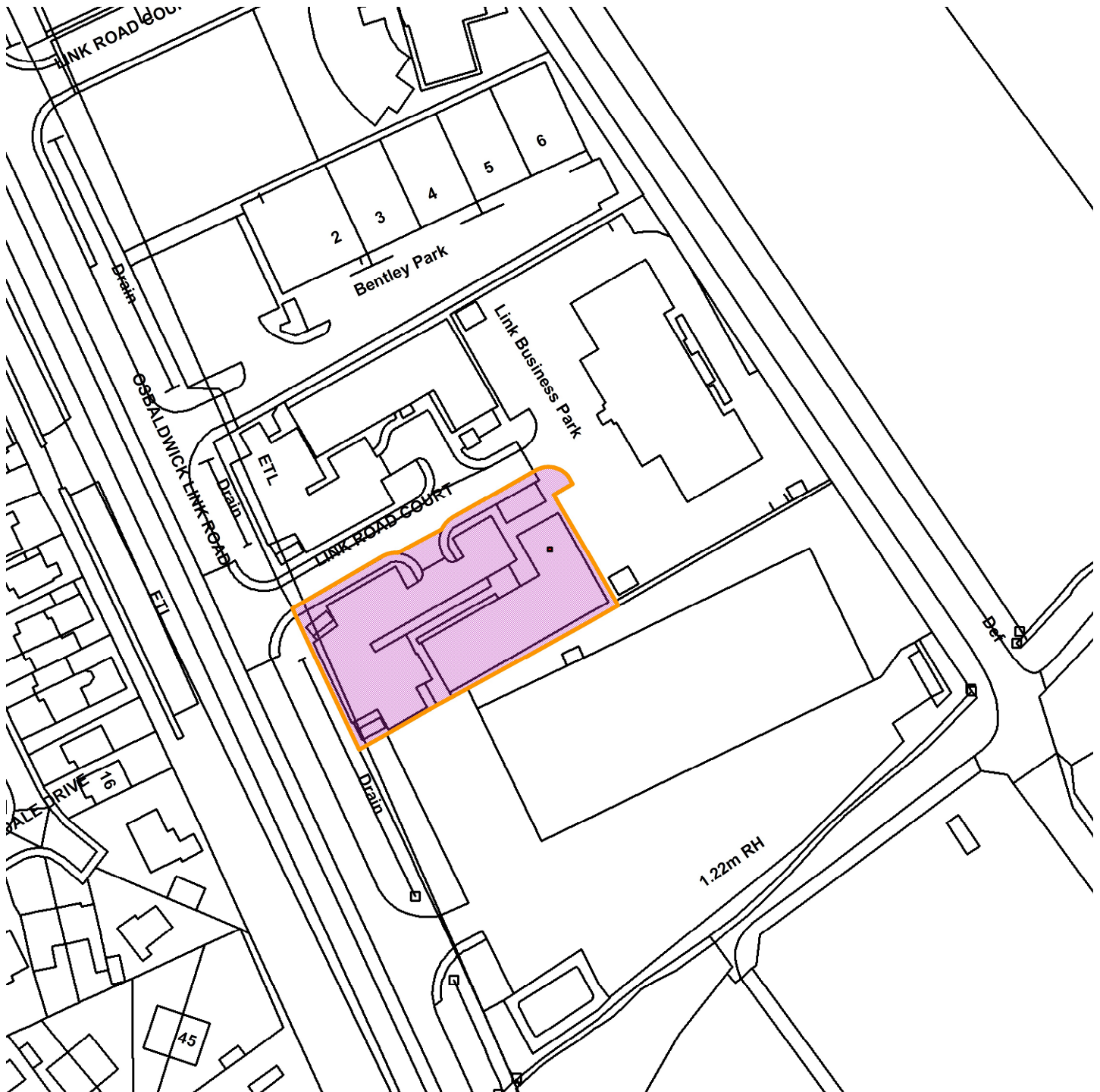
Case Officer: Victoria Bell

Tel No: 01904 551347

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19/02738/FULM

Bacchus House, Link Road Court, Osbaldwick, YO10 3JB



Scale : 1:1227

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Organisation	City of York Council
Department	Economy & Place
Comments	Site Location Plan
Date	11 August 2020
SLA Number	

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19/02738/FULM - Etas Ltd, Bacchus House, Link Road Court,
Osbaldwick Link Road, Osbaldwick, York

Proposed change of use from offices (Use Class B1) to health care unit (Use Class D1), and single storey extension and flat roof canopy to the north west elevation.

Front Elevation



Page 144

Site of Proposed Extension



Existing Car Park (1)



Existing Car park (2)



Access to
Osbalwick Link
Road, Site on Left



Page 148

Neighbouring
buildings, Site on
Right



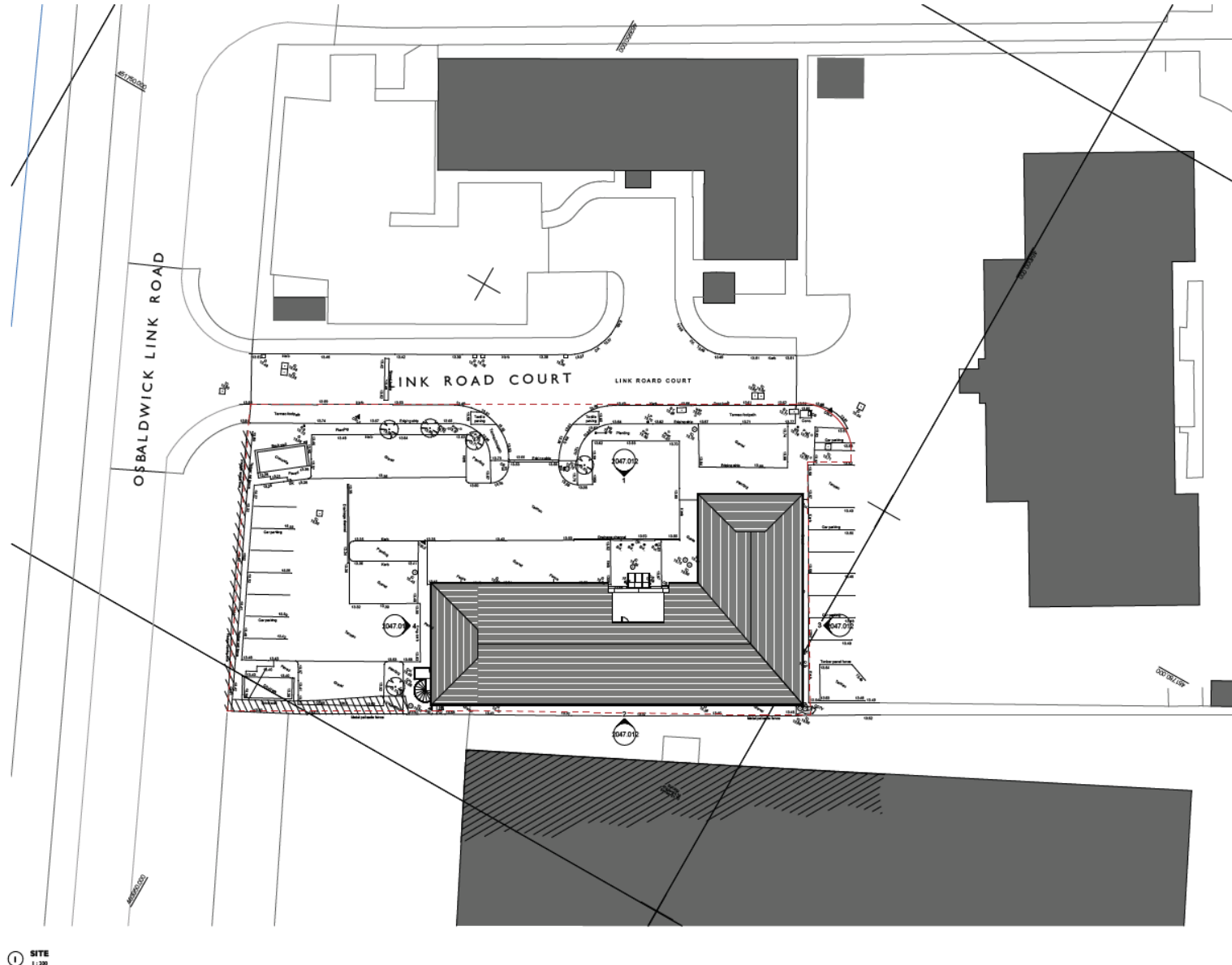
Page 149

Site Location Plan

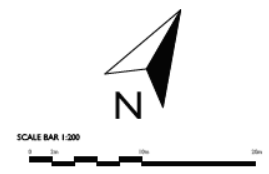


1 SITE TRUE NORTH
1:1250

Existing Site Layout



① SITE
1:100



REVISIONS

NO.	DATE	DESCRIPTION
1		Issue for planning
2		Issue for planning
3		Issue for planning

CLIENT
TEES, ESK & WEAR VALLEY
NHS FOUNDATION TRUST

PROJECT
BACCHUS HOUSE-YORK-YO10 3JB

TITLE
EXISTING_SITE_PLAN
SUBJECT TO SURVEY

DATE
DECEMBER 2019

SCALE
1 : 200

PAPER STATUS
A1 PLANNING

DRAWING NUMBER
2047.010

REVISION

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Tel 01433 304000 Fax 01433 300011 info@richardeves.co.uk
richard eves
ARCHITECTS



Existing Elevations



1 ELEVATION_A
1:100



2 ELEVATION_B
1:100



3 ELEVATION_C
1:100



5 SECTION_I
1:100



REV. A. FULL ADDED TO ELEVATION 1583.20 ASH

REVISIONS	
1	Added existing elevations for guidance in the 3D models and new sections
2	Added Section I
3	Added Section Key
4	Added Section J

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PROJECT
BACCHUS HOUSE-YORK-YO10 3JB

TITLE
EXISTING_ELEVATIONS_&_SECTIONS

DATE
DECEMBER 2019

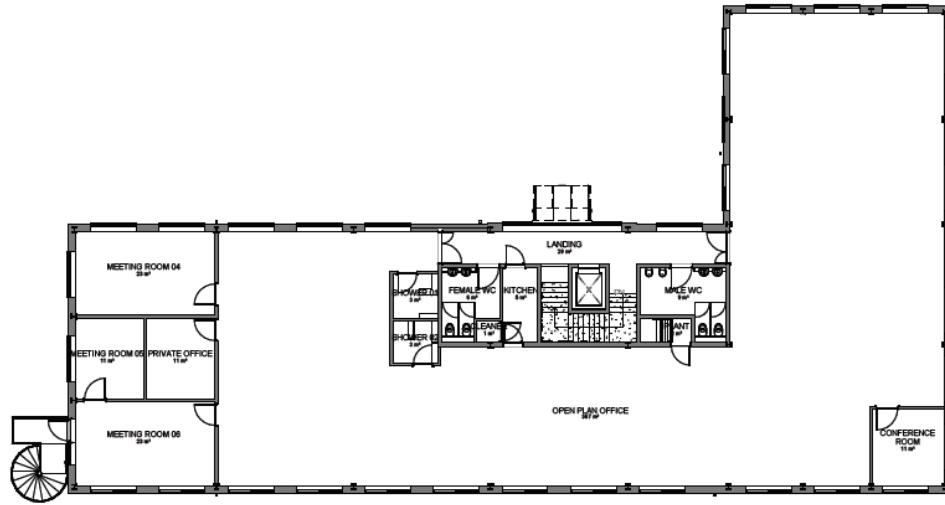
SCALE As indicated PAPER STATUS AI PLANNING

DRAWING NUMBER Richard Eves Architects LLP
2047.012
REVISION A

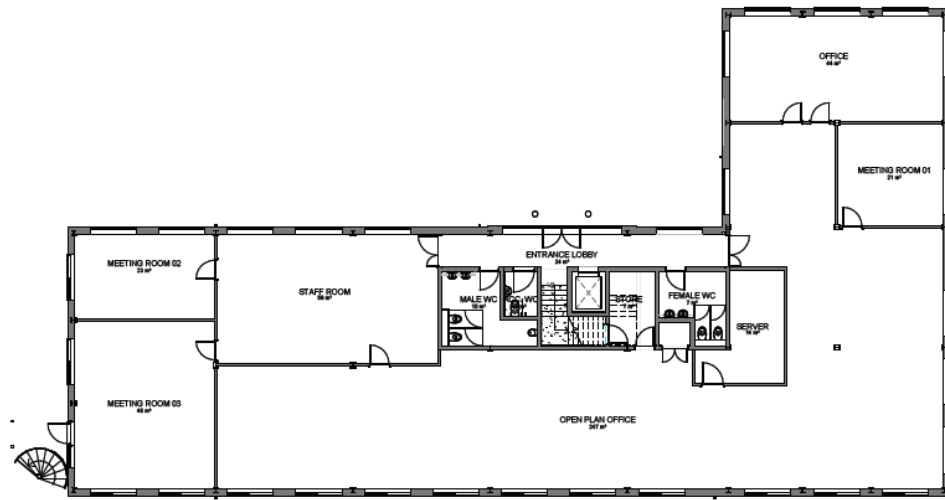
Richard Eves
ARCHITECTS



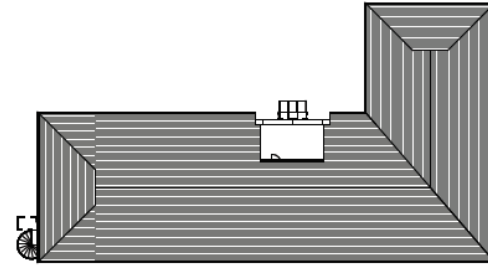
Existing Floor Plans



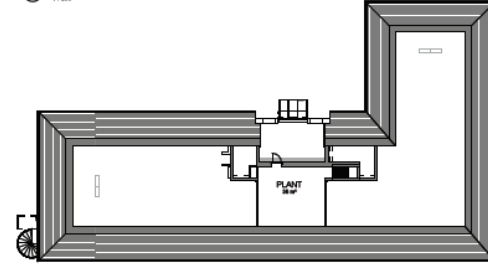
2 01-FIRST_FLOOR
1:100



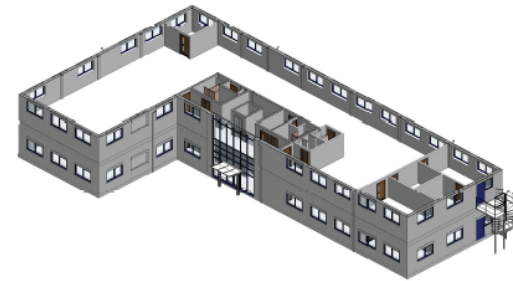
1 00-GROUND_FLOOR
1:100



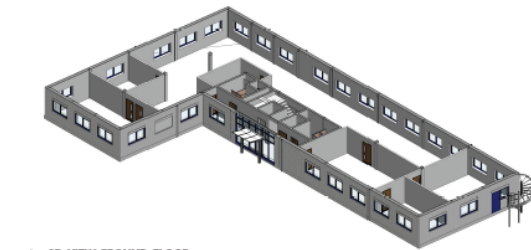
4 R-ROOF
1:100



3 02-SECOND_FLOOR
1:100



4 3D_VIEW-FIRST_FLOOR



3 3D_VIEW-GROUND_FLOOR



SCALE BAR 1:200

SCALE BAR 1:100

REVISIONS

NO.	DATE	DESCRIPTION

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PROJECT

BACCHUS HOUSE-YORK-YO10 3JB

TITLE

EXISTING_FLOOR_PLANS

DATE

DECEMBER 2019

SCALE

As indicated

DRAWING NUMBER

2047.011

REVISION

PAPER STATUS

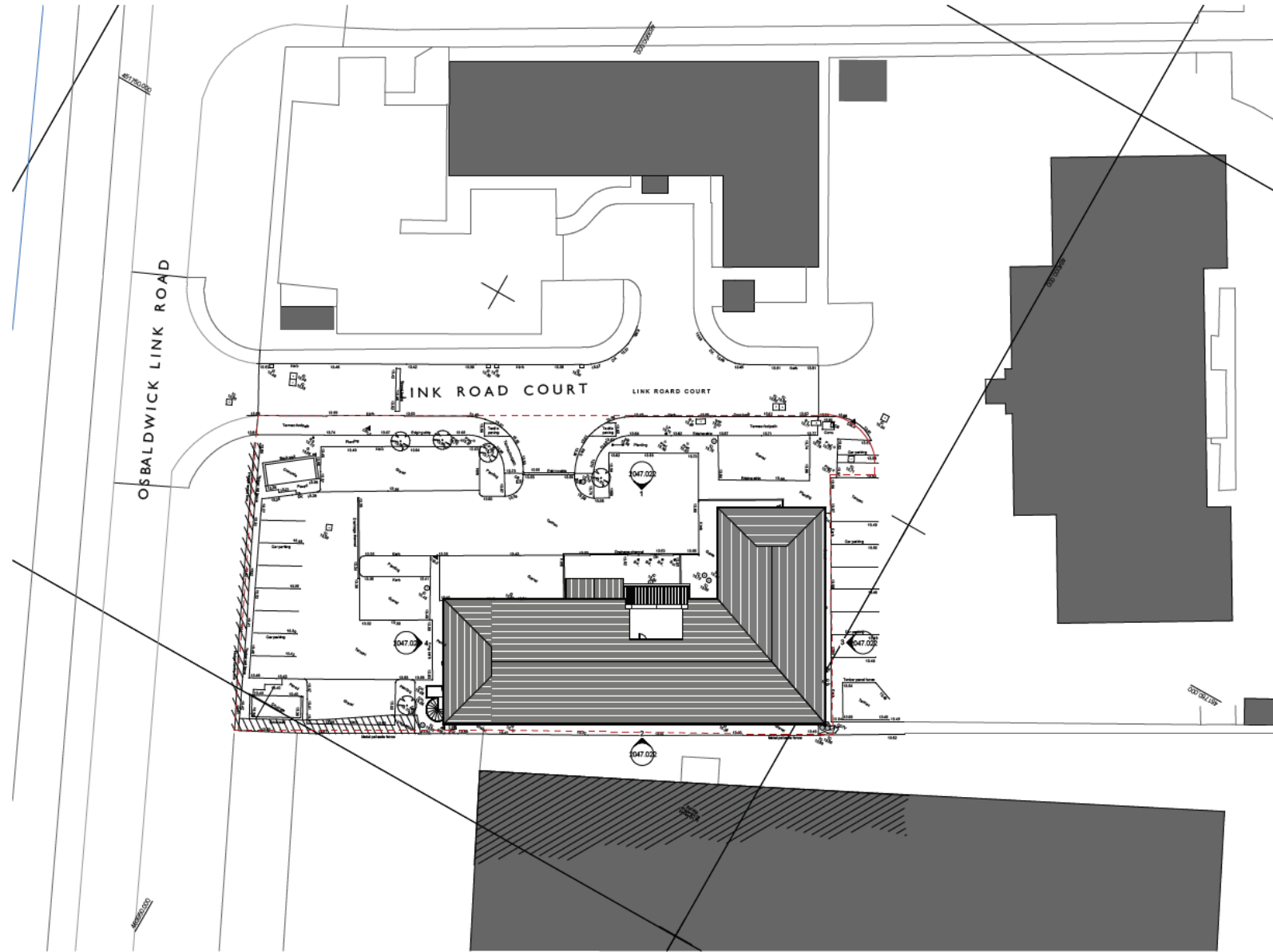
AI PLANNING

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Alameda Villa, 3 Victoria Avenue, Harrogate, HG2 1BQ
Tel: 01423 350468 Fax: 01423 350153 info@richardeves.co.uk

richard eves
ARCHITECTS



Proposed Site Layout



1 SITE
1:200



REVISIONS

NO.	DATE	DESCRIPTION
1		Issue for planning
2		Issue for planning
3		Issue for planning
4		Issue for planning

CLIENT
TEES, ESK & WEAR VALLEY
NHS FOUNDATION TRUST

PROJECT
BACCHUS HOUSE-YORK-YO10 3JB

TITLE
PROPOSED_SITE_PLAN
SUBJECT TO SURVEY

DATE
DECEMBER 2019

SCALE
1:200

PAPER STATUS
A1 PRELIMINARY

DRAWING NUMBER
2047.020

REVISION

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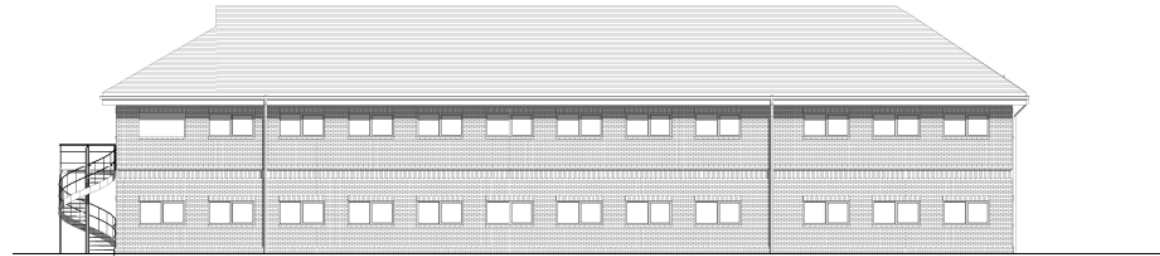
richard eves
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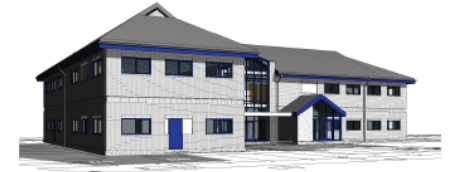
Proposed Elevations



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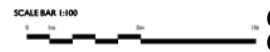
2 ELEVATION_B
1:100



3 ELEVATION_C
1:100



5 SECTION_1-PLANNING
1:100



REV A. FILE ADDED TO SUBMITION 180320 A201

REVISIONS

NO.	DESCRIPTION
1	Issue for planning purposes as an A1 tender submission
2	Issue for planning purposes
3	Issue for planning purposes

CLIENT
TEES, ESK & WEAR VALLEY
NHS FOUNDATION TRUST

PROJECT
BACCHUS_HOUSE-YORK-YO103JB

TITLE
PROPOSED_ELEVATIONS_& SECTIONS

DATE
DECEMBER 2019

SCALE As indicated PAPER STATUS A1 TENDER

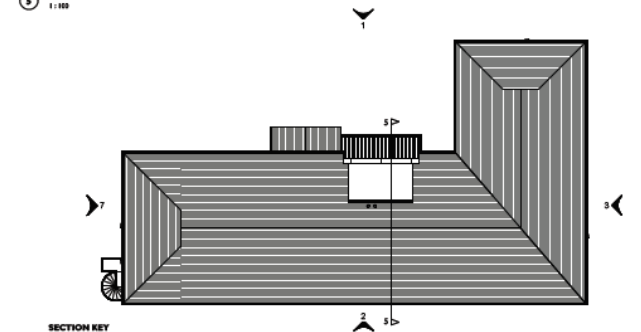
DRAWING NUMBER 2047.022
REVISION A

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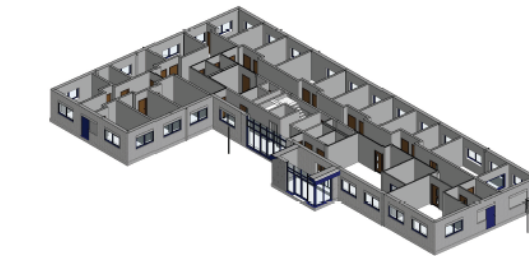
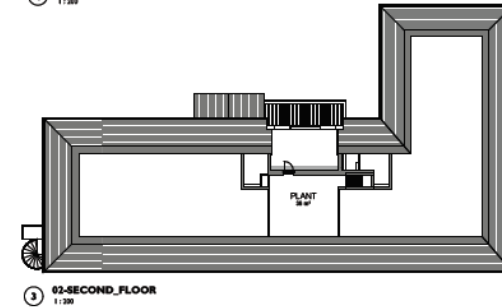
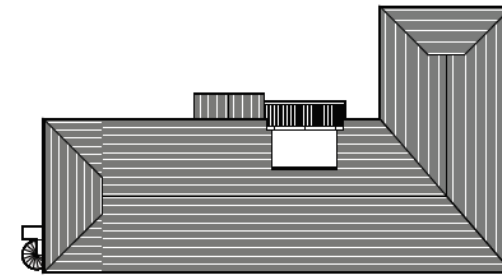
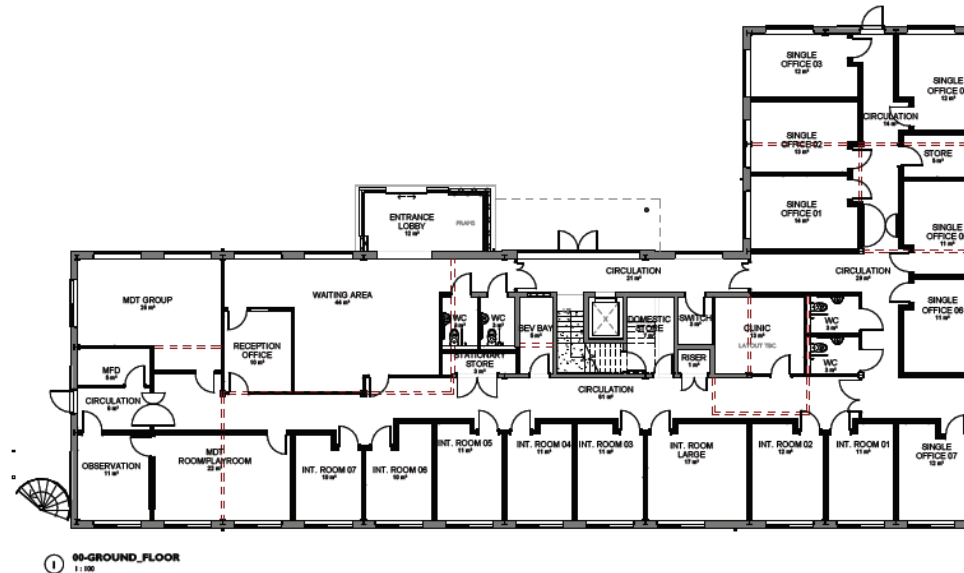
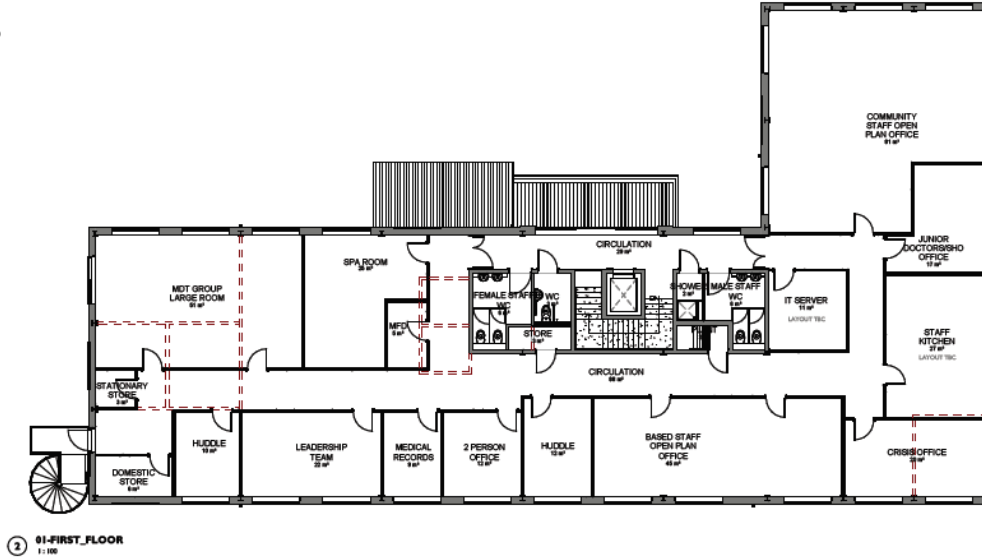
4 ELEVATION_D
1:100



SECTION KEY



Proposed Floor Plans



REV A: LAYOUT APPROVED AT CLIENT REQUEST 20.10.19 ADP/1
REV B: LAYOUT APPROVED AT CLIENT REQUEST 31.10.19 ADP/1

REVISIONS

NO.	DESCRIPTION	DATE
1	As Issued	20.10.19
2	As Issued	31.10.19

CLIENT
TEES, ESK & WEAR VALLEY
NHS FOUNDATION TRUST

PROJECT
BACCHUS HOUSE-YORK-YO10 3JB

TITLE
PROPOSED_FLOOR_PLANS

DATE
DECEMBER 2019

SCALE
As indicated

PAPER STATUS
AI PRELIMINARY

DRAWING NUMBER
2047.021

REVISION
B

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